

Planning Approval Environmental Review Form

SM-22-00008046

Sydney Metro – Metro Body of Knowledge (MBoK)

Assessment Name:	Overhead cable upgrade at Pacific Highway, Crows Nest
Prepared by:	Sydney Metro
Prepared for:	Sydney Metro
Assessment number	TfNSW 74
Type of assessment:	Assessment under EP&A Act 1979, Division 5.2
Version:	Final (1.0)
Planning approval No. (where relevant):	SSI_7400
Date required:	October 2023
iCentral number	SM-23-00854160

Form information – do not alter

Form number	SM-22-00008046
Applicable to:	Sydney Metro
Document Owner:	Director, Planning Approvals
System Owner:	Executive Director, Environment, Sustainability & Planning
Status:	Final
Version:	2.0
Date of issue:	June 2023
Review date:	As required
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Environmental Review

1. Proposed works and justification

An environmental review is applicable to design changes which are consistent with the conditions of approval and would have negligible impacts on the community and/or the environment. This environmental review is required to demonstrate compliance with the conditions of approval and Sydney Metro Chatswood to Sydenham Environmental Impact Statement (EIS) and the Chatswood to Sydenham Submissions and Preferred Infrastructure Report (SPIR). A description of activities is listed in Table 1 and an assessment provided in Section 2.

Table 1 Description of proposed works

Description	Overview
Location of works	<p>The overhead cable wirings are located at 366-398 Pacific Highway, Crows Nest. The proposal area comprises of eleven individual properties within the North Sydney LGA (See Figure 1).</p> <p>The properties are located on the southern side of the Pacific Highway and are divided by Hume Street. The proposal is located directly opposite the project boundary of the Crows Nest Metro Station.</p>
Scope of works	<p>The proposal consists of replacing the existing overhead cables with new cables between 366-398 Pacific Highway including the existing overheads of locally heritage listed Higgins Buildings, using the existing connection at the building façade (See Figure 2). The overheads would be replaced with wiring that look very similar if not the same.</p> <p>A new power pole would also replace an existing pole on Hume Street in the same location and of a consistent materiality (No.376).</p> <p>The works would take one night shift to complete, with a power outage implemented to complete the work.</p> <p>The scope of the works assessed within this Environmental Review include:</p> <ul style="list-style-type: none"> Remove the existing overheads connected to the pole in front of OSD B (RPH) Connect the remaining existing overhead lines in the existing pole (EPI) along Pacific Highway West South of Hume St The removal and reinstatement of a power pole on Hume St. <p>Any consultation (if required) would be carried out by the utility provider.</p>
Justification for works	<p>The proposed works would replace outdated, redundant overhead wiring with 'like for like' upgraded wiring, improving the overall amenity of the site.</p>
Timeframe for works	<p>Works are proposed to occur in a one night shift during February 2024, but it might require a longer period due to unseen circumstances (e.g. weather).</p>
Work hours, workforce and equipment / machinery	<p>The power outage work would be completed during one night shift.</p> <p>Two crews (working on both sides of Pacific Highway) would be required.</p> <p>The below plant equipment would be required for the works:</p> <ul style="list-style-type: none"> Elevated Work Platform Hand tools Cherry Picker

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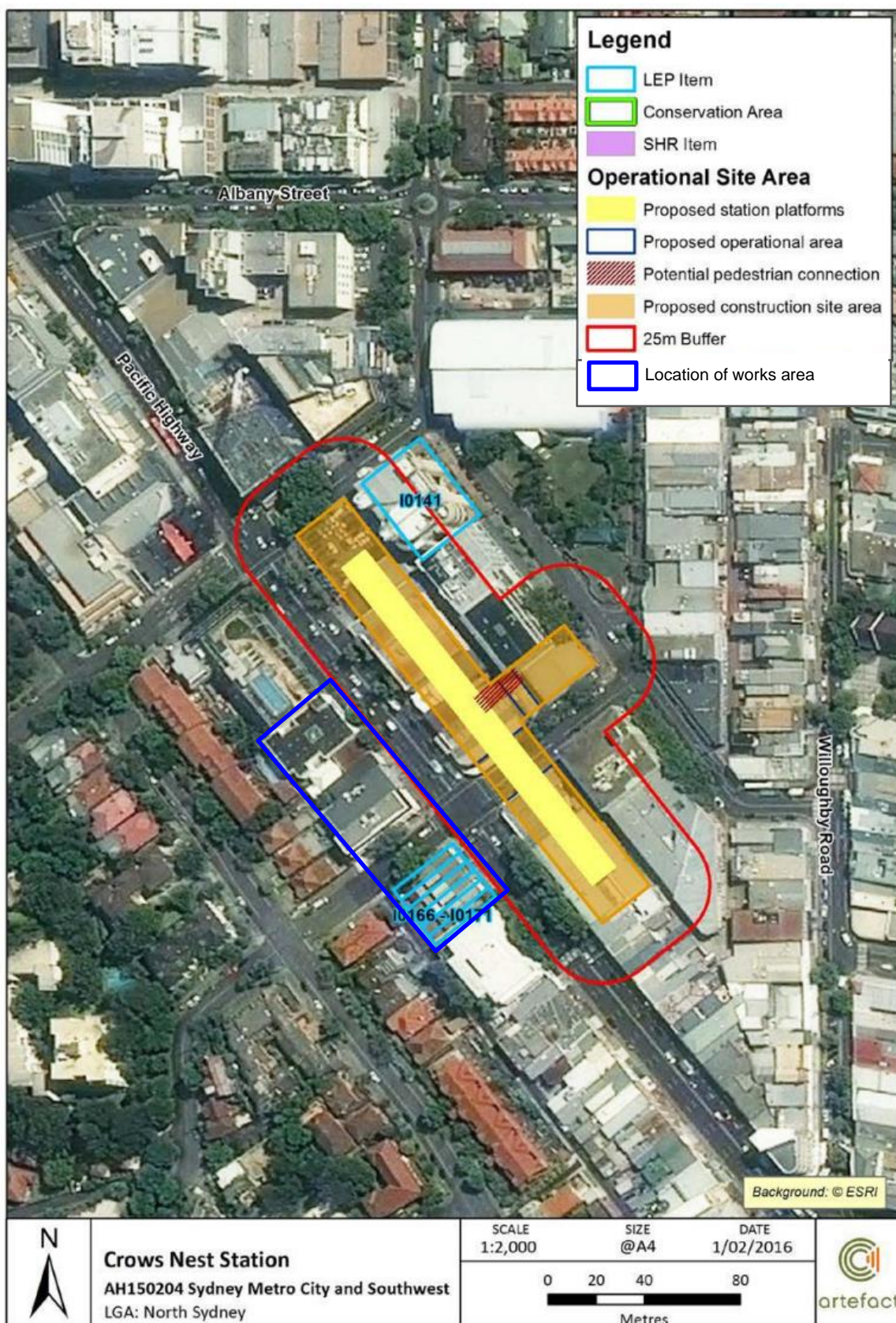


Figure 1 – Location of works area related to construction boundary and Higgins Building (EIS Technical Paper 4)

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Figure 2 – Street view of Heritage Buildings with current overhead wiring circled in blue.

2. Consistency with Conditions of Approval

The following table outlines whether the proposed changes would be consistent with the relevant Conditions of Approval.

Table 2 Comparison of the proposal with relevant elements of the Approved Project

Relevant elements of the Approved Project	Proposed Change
The approved project (SSI 7400) includes a number of ancillary components, including new and alterations to existing overhead wiring, facilities for pedestrians, and other construction related works.	The proposed change is consistent with the EIS. The proposal consists of replacing the existing overhead cables with new cables between 366-398 Pacific Highway, using the existing connection at the building façade.
The approved project also allows for utility works to occur adjacent and outside the project boundary under Section 7.11.5 of the EIS.	The proposed change is consistent with the EIS. The proposal is related to overhead electrical cables and is located directly opposite the project boundary of the Crows Nest Metro Station.
Section 14.5.3 of the EIS assessed negligible indirect impacts to Higgins Buildings, for views and vistas only.	The proposed works comprise the replacement of overhead wiring to the locally significant 'Higgins Buildings' group at 366-376 Pacific Highway. Whilst the EIS considered the visual heritage impact, the physical impact was not assessed.

3. Environmental review

The following table provides a risk review of the potential environmental impacts of the proposed works.

Table 3 Environmental review

Environmental review	Yes / No	Description of impacts (including consideration of safeguards required by the Approved Project)
Is the proposal to take place outside of the construction footprint of the project	Yes	The proposed works are located directly opposite the project boundary of the Crows Nest Metro Station. The approved project allows for utilities work outside of the approved construction boundaries. The proposal is required to replace outdated, redundant overhead wiring and improve the overall amenity of the site.
Is the location of works within the existing EPL premise boundary	No	There is no EPL required to be in place at the Crows Nest ISD construction site.
Will the works take longer than 2 weeks to complete.	No	The works are proposed to occur in less than two weeks.
Does the work require OOHW approval	Yes	The works requires OOHW approval for night-time works.
Will the works impact an EEC or threatened species	No	N/A. No EEC or threatened species applicable to the area.
Will works impact on native vegetation	No	N/A. No clearing of vegetation required.
Will the works impact on habitat trees	No	N/A. No clearing of vegetation required.

Will clearing of non EECs or ground disturbance be of High / moderate condition vegetation. What is the area of impact	No	N/A. No clearing of vegetation required.
Will the works result in medium / high noise or vibration impacts Will noise and vibration impacts on sensitive receivers be greater than that predicted in the EIA	No	The equipment that would be used during the works would include Elevated Work Platform, Hand tools and Cherry Picker which is consistent with the noise levels assessed during EIS from the construction activities (either from station works or outside of station (e.g., Utility)).
Will the works result in medium/ high air quality impacts	No	N/A. Works would not cause any air quality impacts.
Will the activity be located adjacent to or in close proximity to sensitive receivers	Yes	The works would be located close to the sensitive receivers however, the impacts are consistent with what has been assessed during EIS.
Would there be additional impact from what was predicted in the EIS on an Aboriginal / Historic heritage site as a result of the works	No	<p>The EIS considered the visual heritage impact, but did not assess for the physical impacts to building fabric. A Heritage Impact Statement was obtained for heritage advice and is provided in Appendix A.</p> <p>The individual properties from 366-376 Pacific Highway are identified as items of local significance listed on Schedule 5, Part 1 of the North Sydney LEP 2013, as follows:</p> <ul style="list-style-type: none"> Higgins Building (LEP Item No. I0166) 366 Pacific Highway, Crows Nest Higgins Building (LEP Item No. I0167) 368 Pacific Highway, Crows Nest Higgins Building (LEP Item No. I0168) 370 Pacific Highway, Crows Nest Higgins Building (LEP Item No. I0169) 372 Pacific Highway, Crows Nest Higgins Building (LEP Item No. I0170) 374 Pacific Highway, Crows Nest Higgins Building (LEP Item No. I0171) 376 Pacific Highway, Crows Nest. <p>The current overhead wiring is not original fabric; it is considered a neutral amenity that does not contribute to the significance of the heritage items individually or as a group.</p> <p>The proposed works have been designed to involve minimal interventions to the physical fabric of the facades of the individual heritage items. No additional penetrations are proposed for the facades, with existing openings and penetrations utilised for the attachments. Any penetrations left unused as part of the proposal would be made good.</p> <p>The proposal would replace the current wiring without altering the current streetscape presentation of the heritage items.</p> <p>The new power pole replaces an existing pole in the same location and of a consistent materiality and does not alter the streetscape or further impact on the presentation of the northern-most building (No. 376).</p> <p>Overall, the proposal would have a negligible physical impact on the 'Higgins Buildings' group, and no significant further visual impact to the streetscape presentation of the heritage items.</p>
Are works within 10m of a watercourse	No	No watercourses are located within 10m of the location of works.

Are works in an area of known contamination	No	No known contamination exists within the proposed area. Should contamination be identified during works, the existing project contamination procedures would apply.
Will the works result in temporary or long-term traffic impacts	Yes	The works would result in temporary traffic impacts as lane closures would be required along Pacific Highway. All the lanes along Pacific Highway would need to be closed for approximately 10 mins. Traffic and pedestrian management would be implemented in accordance with the CTMP. A road occupancy has been obtained. The works are temporary in nature and limiting construction works to night shift hours only would minimise any traffic related impacts.
Will the works result in visual impacts to sensitive receivers	Yes	Temporary visual impacts would be experienced by surrounding receivers when works are occurring on Pacific Highway. The project would adopt all appropriate mitigation measures to minimise visual intrusiveness to these receivers where possible.
Will the works involve significant earthworks	No	No earth works is required for the for the proposed works.

4. Recommendation

Based on the above assessment, and with reference to the Sydney Metro Chatswood to Sydenham Environmental Impact Statement (EIS) and Chatswood to Sydenham Submissions and Preferred Infrastructure Report (SPIR), including the conditions of approval and associated CEMP and plans, it is recommended that:

✓	The proposed design/construction change is consistent with the Approved Project Sydney Metro Chatswood to Sydenham EIS and Chatswood to Sydenham SPIR including the conditions of approval, has negligible impacts on the community and environment and no further assessment is required.
	The proposed design/construction change is likely to be consistent with the Approved Project Sydney Metro Chatswood to Sydenham EIS and Chatswood to Sydenham SPIR, however more than a negligible impact on the community and environment may result and further assessment in the form of a Planning Approval Consistency Assessment form is required to be completed and submitted to the Planning team for the proposed design/ construction change.
	The proposed design/ construction change is not substantially the same as the Approved Project and is considered a radical transformation. A new planning pathway should be considered.

5. Certification

The above information provides a true and fair review of the proposed works.

Prepared by (signed): *Isabella Caruso*

Date: 12 September 2023

Name: Isabella Caruso

Position: Planning Approval Officer

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6. Endorsement

I have reviewed the above review and provide the following endorsement:

✓	The proposed design/construction change is consistent with the Sydney Metro Chatswood to Sydenham EIS and Chatswood to Sydenham SPIR, has negligible impacts on the community and environment and no further assessment or modification of the planning approval is required.
	The proposed design/construction change is likely to be consistent with the Sydney Metro Chatswood to Sydenham EIS and Chatswood to Sydenham SPIR, however more than negligible impacts are expected on the community and environment and further assessment is required.
	The proposed design/construction change constitutes a project modification and requires further assessment and approval.

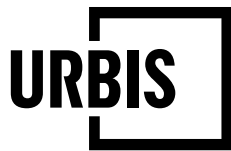
This endorsement is conditional on the following:

1. All works will be carried out in accordance with the Sydney Metro Chatswood to Sydenham Environmental Impact Statement (EIS) and Chatswood to Sydenham Submissions and Preferred Infrastructure Report (SPIR), and the Project Conditions of Approval.
2. All works will be carried out in accordance with the approved Construction Environmental Management Plan and any relevant sub plans.

Signed:	
Endorsed by:	Fil Cerone
Date:	14 September 2023

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Appendix A – Heritage Impact Statement



**ANGEL PLACE
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26 June 2023

Hailey Clonts
Project Manager
Crows Nest Integrated Station Development
Sydney Metro, City and Southwest
C/O Jonathan Garratt, SMEC

To whom it may concern,

366-398 PACIFIC HIGHWAY, CROWS NEST | HERITAGE IMPACT STATEMENT

BACKGROUND

Urbis has been engaged by SMEC to prepare the following Heritage Impact Statement for the proposed upgrade works at 366-398 Pacific Highway, Crows Nest (hereafter referred to as 'the site' or 'the subject site').

The subject site comprises a group of two-storey shop top commercial buildings at 366-398 Pacific Highway, Crows Nest. The site is located within the North Sydney Local Government Area (LGA) and is administered under the provisions of the *North Sydney Local Environmental Plan (LEP) 2013*. The buildings at 366-376 Pacific Highway (within the subject site) are individually identified as local heritage items under Schedule 5, Part 1 of the North Sydney LEP 2013.

As part of the delivery program for the Chatswood to Sydenham Metro project, the proponent is seeking approval to upgrade the overhead wirings at the subject site, replacing the extant redundant wiring 'like for like'. This Heritage Impact Statement provides an assessment of the potential impacts of the proposed upgrade works to the significance of the heritage items.

SITE LOCATION

The subject site comprises the eleven individual properties at 366 – 398 Pacific Highway, Crows Nest within the North Sydney LGA. The properties are located on the southern side of the Pacific Highway and are divided by Hume Street. The subject site is located directly opposite the construction footprint of the Crows Nest Metro Station.

The location of the subject site is shown below.



Figure 1 – Location of subject site indicated in red.

Source: SixMaps

SITE DESCRIPTION

The subject site at 366-398 Pacific Highway in Crows Nest comprises eleven individual commercial dwellings of various periods and styles. The site is set within a commercial strip along the Pacific Highway, a six-lane transport corridor that is one of the main vehicular thoroughfares of North Sydney. The site is located on the southern side of the Pacific Highway directly opposite the construction footprint of the Crows Nest Metro Station.

The site includes a group of locally significant shop-top commercial buildings at 366-376 Pacific Highway. All are designed in the Inter-War Free Classical style and constructed of brick (typically rendered and/ or painted) with suspended metal awnings, rear pitched roofs and highly modified ground floor shopfronts. Their façade is formed as a row of face brick window bays between rendered pilasters, below a rendered parapet with short finials. The parapet features a rendered frieze with projecting centre panel and diamond decoration. Nos. 370-372, at the centre of the group, do not feature pilasters, instead a larger central bay features a triangular pediment, rendered cornice and frieze with the legend 'Higgins Buildings 1923'. No. 376 is located on the corner of the Pacific Highway and Hume Street and features a splayed entry with narrow pediment and parapet, a timber framed shopfront and original leadlight highlights.

The existing wiring attachments are fixed in the centre of each building, with wiring feeding into the interior of each building through a penetration in the parapets. The wiring is considered a neutral non-original material that does not contribute to or detract from the significance of the facades. The wiring and attachments are painted in corresponding colours to each building to ensure the features are recessive when viewed from the streetscape.

The non-significant buildings are divided from the locally significant group by Hume Street. These buildings at 382-398 Pacific Highway, mostly comprise late twentieth century two to three-storey commercial buildings of various styles. The exception is No.388 which also appears to be an Inter-War style shop-top building with modified shopfront currently in use as a party supply store.

The images below provide a visual overview of the subject site.



Figure 2 – View towards subject site from Pacific Highway, looking north. Subject site is indicated in red, grouping of heritage items in teal.

Source: Google Street View



Figure 3 – View towards listed heritage items from Hume Street, looking south. The grouping is indicated in teal.

Source: Google Street View



Figure 4 – View of listed heritage items from Pacific Highway, looking south-west. The grouping is indicated in teal.

Source: Google Street View



Figure 5 – View of non-significant buildings from Pacific Highway, looking south-west. Extent of the subject site is indicated in red.

Source: Google Street View



Figure 6 – View of non-significant buildings from Pacific Highway, looking south-west.

Source: Google Street View



Figure 7 – View of 366-368 Pacific Highway.

Source: Google Street View



Figure 8 – View of 370-372 Pacific Highway.

Source: Google Street View



Figure 9 – View of 374-376 Pacific Highway.

Source: Google Street View

HISTORICAL SUMMARY

The subject site was originally part of the land grant of Edward Wollstonecraft, who arrived in the colony with his sister Elizabeth and brother-in-law Alexander Berry. Wollstonecraft's 548-acre grant on the northern shore of Sydney Harbour was named Crows Nest. Upon his death in 1932, Wollstonecraft's property was inherited by Berry who travelled from his own large holdings on the south coast to reside in Crows Nest.¹ Berry's main house, Crows Nest House, was completed in 1850.

After Elizabeth's death in 1845, Berry began to subdivide his North Shore holdings, beginning with the area near Lane Cove Road (today's Pacific Highway). Sales were sluggish, even after the completion of the North Shore Railway Line south-west of the subject site in the early 1890s. The subject site was part of the estate transferred to Sir John Hay, with subdivisions continuing after his own death in 1909.

¹ North Sydney Council. From Land Grant to Subdivision. Accessible via: file:///C:/Users/artefact/Downloads/From_Land_Grant_to_Subdivision.pdf

The Higgins Buildings were constructed in 1923, intended to house local businesses on the ground floor and tenants on the upper floor. The grouping is clearly visible on a 1947 aerial photograph of Crows Nest.

The ground floor of the Higgins Buildings has been exclusively utilised as commercial buildings since their construction, with advertisements showing that hairdressers, real estate agencies and fruit shops have all occupied the premises. It is likely that the first floor was utilised by tenants affiliated or separate from the businesses operating on the ground floor.

Non-significant development on the northern portion of the site appears to be largely late 20th century commercial buildings, some of which replaced earlier development, as indicated in the 1943 aerial below.



Figure 10 – Aerial photograph of subject site from 1943, with location of subject site indicated in red and the grouping indicated in teal.

Source: SixMaps

HERITAGE SIGNIFICANCE

Heritage Listing

The individual properties from 366-376 Pacific Highway are identified as items of local significance listed on Schedule 5, Part 1 of the North Sydney LEP 2013, as follows:

- “Higgins Building” (LEP Item No. I0166) 366 Pacific Highway, Crows Nest
- “Higgins Building” (LEP Item No. I0167) 368 Pacific Highway, Crows Nest
- “Higgins Building” (LEP Item No. I0168) 370 Pacific Highway, Crows Nest
- “Higgins Building” (LEP Item No. I0169) 372 Pacific Highway, Crows Nest

- “Higgins Building” (LEP Item No. I0170) 374 Pacific Highway, Crows Nest
- “Higgins Building” (LEP Item No. I0171) 376 Pacific Highway, Crows Nest.

The heritage context of the subject site is shown below.

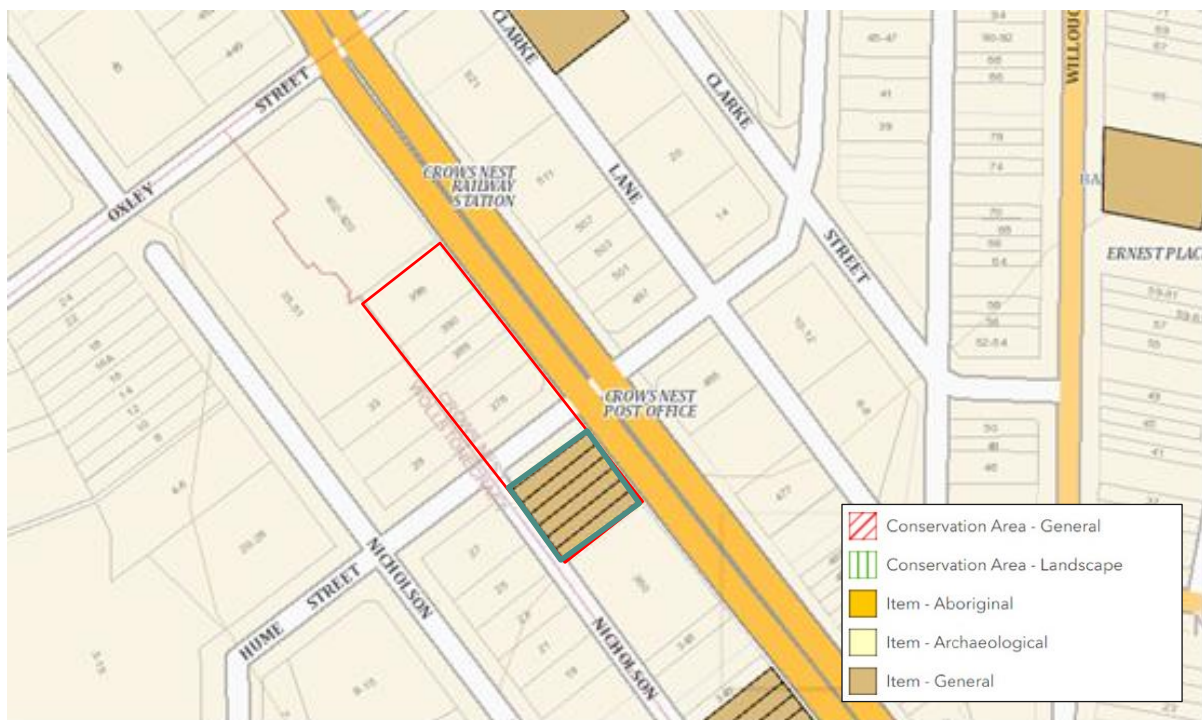


Figure 11 – Extract of heritage map with the subject site outlined in red and grouping outlined in teal.

Source: NSW Planning Portal, ePlanning Spatial Viewer

Statement of Significance

The statement of significance for the Higgins Buildings group, as extracted from the State Heritage Inventory, is as follows:

An example of a two storey brick and rendered masonry commercial building in the Inter War Free Classical style. The group is a good example of an Interwar commercial/retail building housing a group of shops which occupies an important corner location and which complements and reflects the type of development characteristic of this streetscape.²

² State Heritage Inventory, n.d. “Higgins Buildings”. Accessible via:
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2181758>

PROPOSED WORKS

Urbis understands that the proponent is seeking approval to upgrade the overhead wirings at the subject site, replacing the extant wiring like for like. This includes the replacement of a pole on Hume Street.

Urbis received the design drawings from SMEC on 15 June 2023, which form the basis of this assessment. Selected plans are included overleaf; please refer to full design package for full size plans.

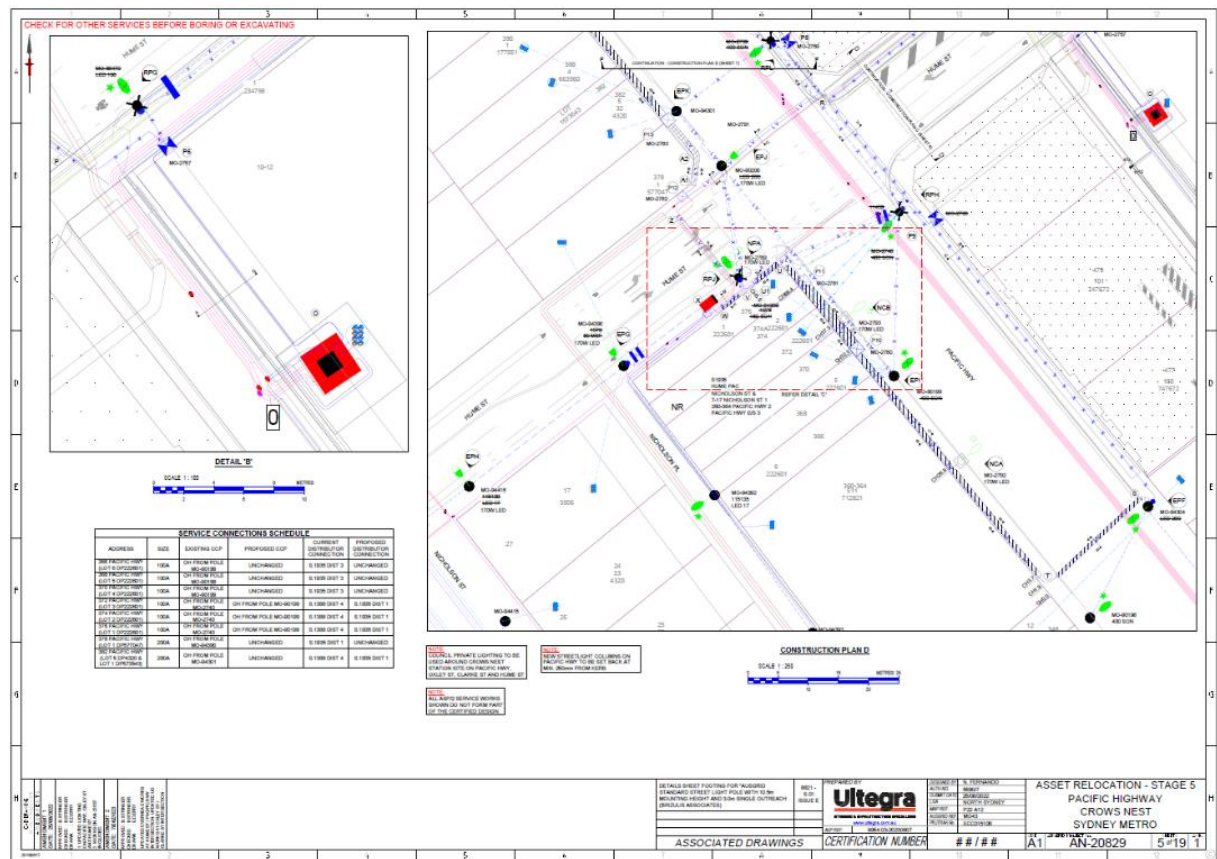


Figure 12 – Asset relocation plans, AN-20829.

Source: Ultegra via SMEC

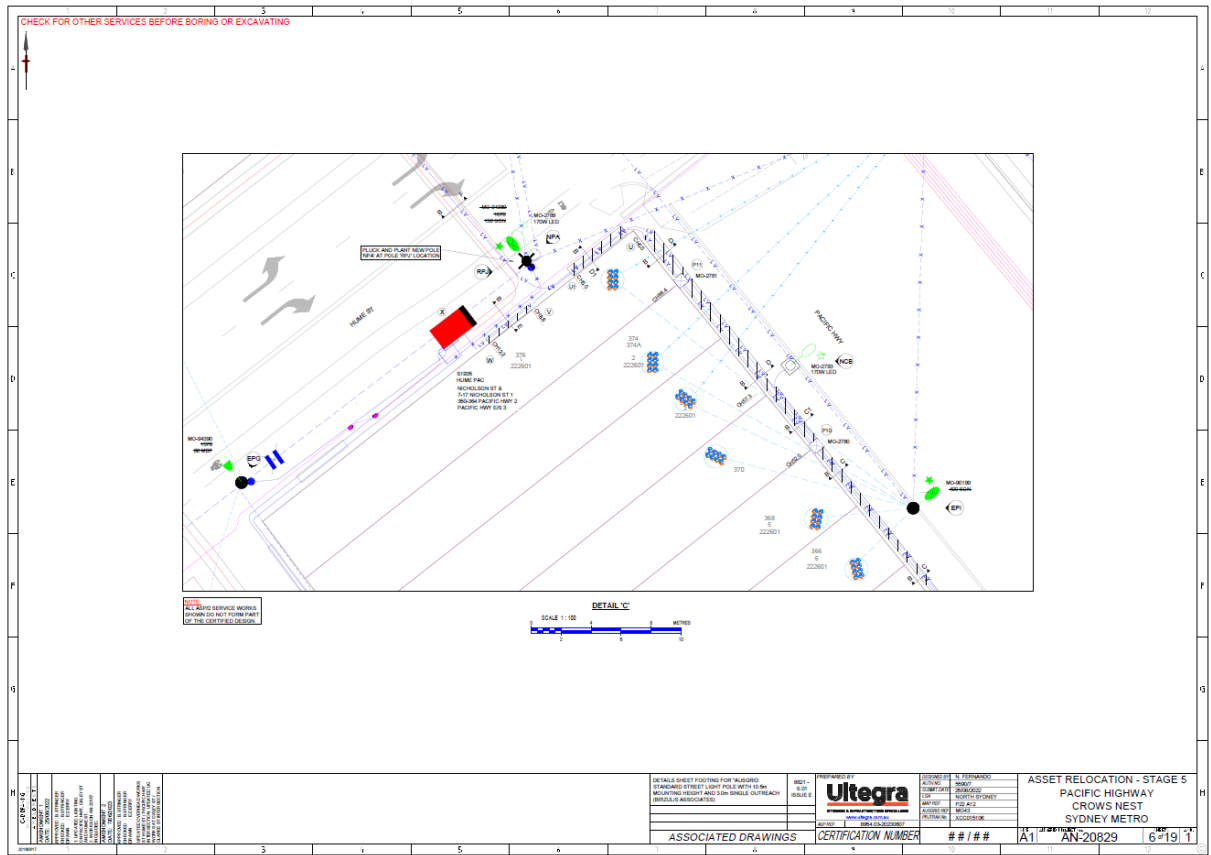


Figure 13 – Asset relocation plans at the subject site, AN-20829.

Source: Ultegra via SMEC

ASSESSMENT OF HERITAGE IMPACT

The following assessment addresses the potential heritage impacts of the proposed overhead wirings upgrade on the heritage significance of 366-376 Pacific Highway, including building fabric and streetscape presentation. The assessment is based on the relevant statutory and non-statutory planning controls in the North Sydney LEP 2013 and Appendix 1 'Heritage' of the North Sydney Development Control Plan (DCP) 2013.

The proposed works comprise the replacement of redundant overhead wiring to the commercial buildings at 366-398 Pacific Highway, including the locally significant 'Higgins Buildings' group at 366-376 Pacific Highway (LEP Item Nos. I0166 - I0171). The overhead wirings, including attachment and fixtures, will be replaced 'like for like' with similar materials, dimensions, locations and fixings proposed that are consistent with the extant overhead wirings.

The current overhead wiring is not original fabric; it is considered a neutral amenity that does not contribute to the significance of the heritage items individually or as a group. The proposed would replace outdated, redundant overhead wiring with 'like for like' upgraded wiring, improving the overall amenity of the site.

The proposed works have been designed to involve minimal interventions to the physical fabric of the facades of the individual heritage items. No additional penetrations are proposed for the facades, with existing openings and penetrations utilised for the attachments. Any penetrations left unused as part of the proposal would be made good. Urbis understands that fixings are consistent with the existing. The proposed works are considered minimal and would have a very negligible and acceptable physical impact on the 'Higgins Buildings' group.

The current wiring is visible from the Pacific Highway and Hume Street elevations of the building, and is considered part of the streetscape presentation of the 'Higgins Buildings' group. Similarly, the proposal would replace the current wiring without altering the current streetscape presentation of the heritage items. Urbis recommends that any casing or conduit for proposed wiring be painted in corresponding colours to each building to ensure that the proposal appears recessive when viewed from the streetscape and blends with the respective building facades.

The new power pole replaces an existing pole in the same location and of a consistent materiality and does not alter the streetscape or further impact on the presentation of the northern-most building (No. 376) which has a secondary frontage to Hume Street.

Overall, the proposal would have a negligible physical impact on the 'Higgins Buildings' group, and no significant further visual impact to the streetscape presentation of the heritage items.

CONCLUSION

The proposed development, comprising the replacement of redundant overhead wiring and new street pole, will have no significant impact on the heritage significance of the 'Higgins Buildings' group.

In light of the above considerations, Urbis has no hesitation in supporting the proposed works and recommends approval from a heritage perspective. For further discussion, don't hesitate to contact the undersigned.

Kind regards,

A handwritten signature in dark ink, appearing to read "Darrienne Wyndham". The signature is fluid and cursive, with the first name being more prominent.

Darrienne Wyndham
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