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Sydenham Metro Upgrade Project Heritage Interpretation Plan

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1. Introduction

1.1 The Brief

Extent Heritage Pty Ltd was commissioned by Purcell to prepare a Heritage Interpretation Plan (HIP) for the Sydenham Metro Upgrade Project. The Brief for the Interpretation Strategy and Plan for the Sydenham Metro Upgrade Project is outlined within the projects Scope of Works and Technical Criteria (SWTCs) Document Reference 02.01.04.04.06.01 Schedule C1 Appendix B6. The scope of works includes the following:

Identification and interpretation of the key Aboriginal and Non-Aboriginal heritage values and stories of heritage items and heritage conservation areas impacted. This must be included in the Heritage Plan, and prepared in accordance with:

- a. the NSW Heritage Manual;
- b. the NSW Heritage Office's Interpreting Heritage Places and Items: Guidelines (August 2005); and
- c. the NSW Heritage Council's Heritage Interpretation Policy.

This HIP relates to Condition E21 of the conditions of approval which states:

The Proponent must prepare a Heritage Interpretation Plan which identifies and interprets the key Aboriginal and Non-Aboriginal heritage values and stories of heritage items and heritage conservation areas impacted by the CSSI. The Heritage Interpretation Plan must inform the Station Design and Precinct Plan referred to in Condition E101. The Heritage Interpretation Plan must be prepared in accordance with the NSW Heritage Manual, the NSW Heritage Office's Interpreting Heritage Places and Items: Guidelines (August 2005), and the NSW Heritage Council's Heritage Interpretation Policy and include, but not be limited to:

- a. a discussion of key interpretive themes, stories and messages proposed to interpret the history and significance of the affected heritage items and sections of heritage conservation areas including, but not limited to the Sydney Terminal and Central Railway Stations Group, Martin Place Station, Sydenham Station and Sydenham Pit and Drainage Pumping Station Precincts;
- b. identification and confirmation of interpretive initiatives implemented to mitigate impacts to archaeological Relics, heritage items and conservation areas affected by the CSSI including:
 - i. use of interpretive hoardings during construction
 - ii. community open days

- iii. community updates
- iv. station and precinct design; and
- c. Aboriginal cultural and heritage values of the project area including the results of any archaeological investigations undertaken.

The Heritage Interpretation Plan must be prepared in consultation with the Heritage Council of NSW (or its delegate), relevant councils and registered Aboriginal parties, and must be submitted to the Secretary before commencement of construction.

The STWCs go on to define the following specific requirements of the staged Heritage Interpretation Plan:

The SSJ Contractor must prepare and submit, for approval of the Principal’s Representative, a Heritage Interpretation Plan (HIP). The HIP must document the story of the development of the Bankstown line through to the present day. The plan must then detail the history of the Sydenham Station and Sydenham Pit, and their contribution to the development of both the Bankstown line and the surrounding suburb. The Heritage Interpretation Plan must include a review of all existing interpretation so that a consistent approach can be developed for Sydenham Station precinct.

Interpretation is likely to be undertaken in a number of stages, in accordance with accepted methodology. It would include the following:

- Stage 1 would comprise the preparation of an interpretation strategy, based on historic research, development of themes, identifying potential audiences and possible media formats, and preliminary concept development;
- **Stage 2, The Heritage Interpretation Plan would comprise content development, formulation of text and selection of images, and the design of the media or installations;**
- Stage 3 would comprise the actual installation of the interpretive media. Refer to the document ‘Heritage Interpretation Strategy—Southwest Stations and Corridor Works’ (GML Heritage, March 2017).

This HIP addresses Stage 2 interpretation and will be followed by a detailed design and implementation process as part of the construction package.

1.2 Conditions of Approval

Table 1. Conditions of Approval.

Requirement of the conditions of approval	Where requirements are addressed in the plan
<p>Condition E21</p> <p>The Proponent must prepare a Heritage Interpretation Plan which identifies and interprets the key Aboriginal and Non-Aboriginal heritage values and stories of heritage items and heritage</p>	<p>This HIP partially fulfils the requirements of this Condition, specifically with reference to</p>

Requirement of the conditions of approval	Where requirements are addressed in the plan
<p>conservation areas impacted by the CSSI. The Heritage Interpretation Plan must inform the Station Design and Precinct Plan referred to in Condition E101. The Heritage Interpretation Plan must be prepared in accordance with the NSW Heritage Manual, the NSW Heritage Office’s Interpreting Heritage Places and Items: Guidelines (August 2005), and the NSW Heritage Council’s Heritage Interpretation Policy and include, but not be limited to:</p> <ul style="list-style-type: none"> d. a discussion of key interpretive themes, stories and messages proposed to interpret the history and significance of the affected heritage items and sections of heritage conservation areas including, but not limited to the Sydney Terminal and Central Railway Stations Group, Martin Place Station, Sydenham Station and Sydenham Pit and Drainage Pumping Station Precincts; e. identification and confirmation of interpretive initiatives implemented to mitigate impacts to archaeological Relics, heritage items and conservation areas affected by the CSSI including; <ul style="list-style-type: none"> i. use of interpretative hoardings during construction ii. community open days iii. community updates iv. station and precinct design; and f. Aboriginal cultural and heritage values of the project area including the results of any archaeological investigations undertaken. <p>The Heritage Interpretation Plan must be prepared in consultation with the Heritage Council of NSW (or its delegate), relevant councils and registered Aboriginal parties, and must be submitted to the Secretary before commencement of construction.</p>	<p>Sydenham Station and Sydenham Pit and Drainage Pumping Station. As this HIP has been prepared prior to the archaeological program, any significant archaeological remains uncovered during works will need to be assessed for inclusion during the detailed design and implementation of the HIP to be delivered as part of the construction package. Consultation with relevant RAPs has been undertaken and the findings included in this report.</p> <p>History—refer to Section 4.</p> <p>Themes—refer to Section 5.</p> <p>Consultation – refer to Section 6 and Appendix 2.</p> <p>Interpretation initiatives/devices—refer to Section 7 and Appendix 1.</p> <p>Aboriginal cultural and heritage values—refer to Section 4.</p>
<p>REMM NAH2</p> <p>Significant archaeological findings would be considered for inclusion in heritage interpretation (as per NAH8) for the project and be developed in consultation with the relevant local council.</p>	<p>As this HIP has been prepared prior to the archaeological program, any significant archaeological remains uncovered during works will need to be assessed for inclusion during the interpretation implementation delivery which is included in the construction package.</p>
<p>REMM NAH8</p> <p>Appropriate heritage interpretation would be incorporated into the design for the project in accordance with the NSW Heritage Manual, the NSW Heritage Office’s Interpreting Heritage Places and Items: Guidelines (August 2005), and the NSW Heritage Council’s Heritage Interpretation Policy.</p>	<p>This HIP fulfils this requirement.</p> <p>Methodology—refer to Section 1.3</p>

Requirement of the conditions of approval	Where requirements are addressed in the plan
	Interpretation initiatives/devices—refer to Section 7 and Appendix 1.
REMM NAH4	
Appropriate Aboriginal heritage interpretation would be incorporated into the design for the project in consultation with Aboriginal stakeholders.	Aboriginal cultural and heritage values—refer to section 4. Consultation - refer to Section 6 and Appendix 2.

1.3 Methodology

This report has been prepared by a multidisciplinary team with a wide range of experience in interpretation planning. In developing this report, the Extent Heritage team has worked collaboratively with Purcell who are managing the built heritage reporting for the Sydenham Metro Upgrade Project and HASSELL + Weston Williamson who are preparing the project design, focusing on integrating and relating history and heritage values of the station and junction land with the proposed design and layout of the site.

This HIP has been prepared in accordance with the NSW Heritage Manual, the NSW Heritage Office 'Interpreting Heritage Places and Items: Guidelines' (2005) and the NSW Heritage Council 'Heritage Interpretation Policy' (2005). This includes identifying themes that are appropriate for the setting, undertaking detailed historical analysis to identify important stories and stakeholder consultation. In line with the guidelines the program has attempted to make reasoned choices regarding the stories to be told and to provide a variety of media that will hopefully stimulate interest in the developmental history of the place.

This HIP includes the following key components:

- overview of the place, heritage status, significance and history (see Appendices);
- high-level customer usage data;
- discussion of the key heritage themes which should be interpreted; and
- provision of recommended interpretive devices, with detail on installation locations, preliminary specifications, visual media and content where relevant.

The report draws on additional documentation prepared by Purcell (authors of built heritage reporting), John Holland Laing O'Rourke (JHLORJV) (head contractor) and Extent Heritage for the Sydenham Metro Upgrade Project, including the 'Construction Heritage Management Plan' (2018) and 'Heritage Interpretation Strategy' (2018).

The site was inspected and photographed by the authors of this report in March and August 2018.

1.4 Objectives

This HIP has the following interpretation objectives:

- to develop appropriate devices to address the different needs and interests of the identified audience outlined in the Heritage Interpretation Strategy;
- to improve and enhance audience enjoyment and understanding of the cultural significance of the subject site;
- to provide a mechanism for residents and specialist groups to experience and learn more about the station and junction land;
- to demonstrate best practice interpretation consistent with relevant state, national and international standards and guidelines; and
- to assist in mitigating the heritage impact of the Sydenham Metro Upgrade Project.

1.5 Limitations

The historical overview provides sufficient historical background to provide an understanding of the place in order to provide relevant recommendations; however, it is not intended as an exhaustive history of the site.

The HIP is limited to interpreting activities that were undertaken on, or immediately adjacent to, the station and junction site. It does not seek to include theme or general historical information about surrounding suburbs or streets.

1.6 Authorship

This report has been prepared by Graham Wilson (Senior Heritage Advisor) and Corinne Softley (Heritage Advisor) of Extent Heritage, with assistance from Lucy Burke-Smith of Purcell.

1.7 Terminology

The Burra Charter

The terminology in this report follows definitions presented in *The Burra Charter*. Article 1 provides the following definitions:

Interpretation means all the ways of presenting the cultural significance of a place.

Place means site, area, land, landscape, building or other work, group of buildings or other works, and may include components, contents, spaces and views.

Cultural significance means aesthetic, historic, scientific, social or spiritual value for past, present or future generations.

Cultural significance is embodied in the place itself, its fabric, setting, use, associations, meanings, records, related places and related objects.

Places may have a range of values for different individuals or groups.

Fabric means all the physical material of the place including components, fixtures, contents, and objects.

Conservation means all the processes of looking after a place so to retain its cultural significance.

Maintenance means the continuous protective care of the fabric and setting of a place, and is to be distinguished from repair. Repair involves restoration or reconstruction.

Preservation means maintaining the fabric of a place in its existing state and retarding deterioration.

Restoration means returning the existing fabric of a place to a known earlier state by removing accretions or by reassembling existing components without the introduction of new material.

Reconstruction means returning the place to a known earlier state and is distinguished from restoration by the introduction of new material into the fabric.

Adaptation means modifying a place to suit the existing use or a proposed use.

Use means the functions of a place, as well as the activities and practices that may occur at the place.

Compatible use means a use that respects the cultural significance of a place. Such a use involves no, or minimal, impact on cultural significance.

Setting means the area around a place, which may include the visual catchment.

Related place means a place that contributes to the cultural significance of another place.

The Ename Charter

The terminology in this report also follows definitions presented in *The Ename Charter*.

Interpretation refers to the full range of potential activities intended to heighten public awareness and enhance understanding of cultural heritage site. These can include print and electronic publications, public lectures, on-site and directly related off-site installations, educational programmes, community activities, and ongoing research, training, and evaluation of the interpretation process itself.

Presentation more specifically denotes the carefully planned communication of interpretive content through the arrangement of interpretive information, physical access, and interpretive infrastructure at a cultural heritage site. It can be conveyed through a variety of technical means, including, yet not requiring, such elements as informational panels, museum-type displays, formalized walking tours, lectures and guided tours, and multimedia applications and websites.

Interpretive infrastructure refers to physical installations, facilities, and areas at, or connected with a cultural heritage site that may be specifically utilised for the purposes of interpretation and presentation including those supporting interpretation via new and existing technologies.

Site interpreters refer to staff or volunteers at a cultural heritage site who are permanently or temporarily engaged in the public communication of information relating to the values and significance of the site.

Cultural Heritage Site refers to a place, locality, natural landscape, settlement area, architectural complex, archaeological site, or standing structure that is recognized and often legally protected as a place of historical and cultural significance.

2. Project Context

2.1 Location

Sydenham Station is positioned on the northwest boundary of the suburb of Sydenham within the Inner West Local Government Area (LGA). The site is located between Railway Parade to the north and Burrows Avenue to the south, with the primary station entrance located on Gleeson Avenue to the west. The area is legally defined as Lot 11 in DP 862287. The railway line and station are located within the suburb of Sydenham, while the pump and pit are located within the suburb of Marrickville. This portion of Marrickville was formerly known as Tramvale.

Sydenham Station is a substantial rail precinct. Orientated to the northeast-southwest, the station consists of six platforms (two island and two side) with an associated station building on each, an overhead booking office, brick perimeter walls, footbridge and stairs, and the Gleeson Avenue Overbridge.

Included within the project footprint is the Sydenham Pit and Pump, a water detention and distribution facility operated by Sydney Water. This facility consists of a stone-lined detention pit with associated concrete channels and an elevated pumping station.

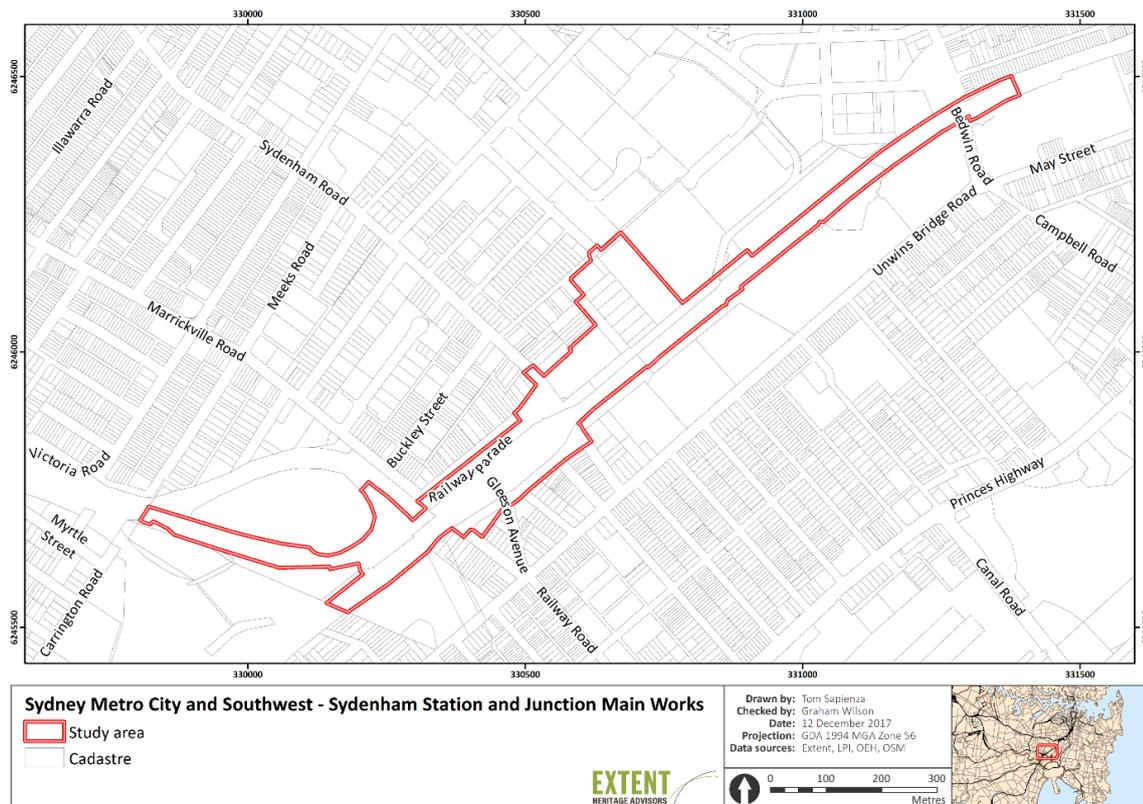


Figure 1. Project site.

2.2 Heritage Status and Significance

Heritage listings

The Sydenham Metro Upgrade Project area contains three listed heritage items and is in close proximity to a number of others. The listed sites within, and adjacent to the project area are identified in the figure below. It should be noted that the curtilage for the Sydenham Station Group differs between the LEP listing and the SHR listing. The SHR listing specifically includes the Gleeson Avenue overbridge.

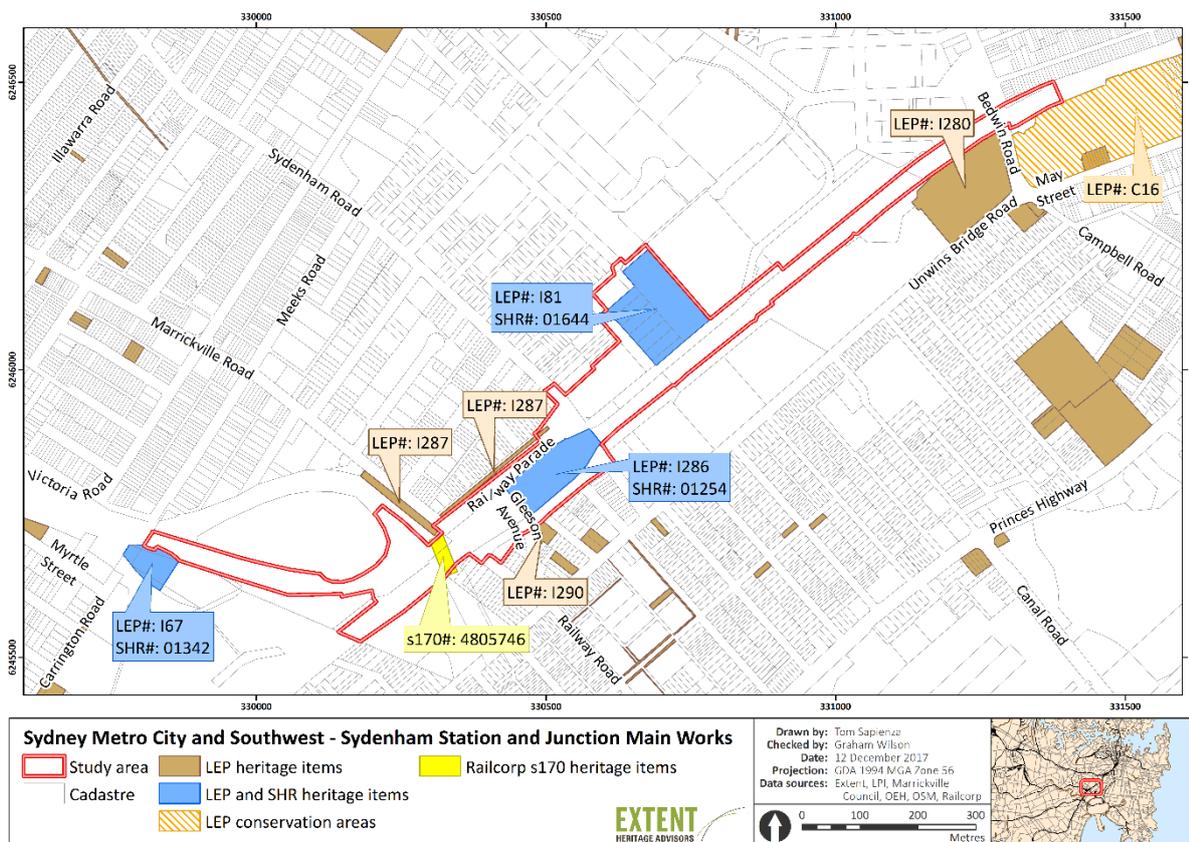


Figure 2. LEP heritage listings.

Significance

Aboriginal cultural heritage

No Aboriginal objects or sites have been previously recorded within the project area, though areas of moderate to high Aboriginal archaeological potential have been identified. The significance of the potential archaeological resources has been based on a preliminary assessment of the archaeological potential, and would be further clarified following excavation, if required. The project area retains potential for intact, deep residual deposits of the Birrong

Soil Landscape which may be of considerable antiquity (greater than 10,000 years), to a depth of 7.5 m below the present ground surface. Aboriginal sites in this region are a rare occurrence and, if present, have the potential to have moderate to high scientific value and high research potential.

No specific cultural value has been identified by the RAPs in relation to the project area. However, sites of potential antiquity, and which contain extensive cultural material, are frequently identified as being of importance to Aboriginal people, and as such the project area can be considered to have moderate to high overall Aboriginal heritage significance.

The site is located within the Bulanaming district that extended from present day Newtown to Cooks River. Cooks River and its tributaries were significant food resources. Traditional Aboriginal life linked strongly to the river and stream system survived into the 1830s.

Sydenham Station

The existing statement of significance for Sydenham Station is as follows:

Sydenham Station—inclusive of all platform buildings and awnings, parcels office, waiting shed, brick faced platforms, Gleeson Avenue over- bridge and brick perimeter walls—is of State heritage significance. Sydenham Station is of historical significance as a major junction station developed from 1884 to the present, with two 1884 platform buildings, 1925 platform building and waiting shed, 1962 parcels office, and 1920s Gleeson Avenue overbridge demonstrating its development over time, including the adaptation of the 1884 wayside platform buildings for island platform use.

Of aesthetic and historical significance, the platform building awnings demonstrate the range of awnings used on railway buildings from the small original awning of two bays on the Platform 2/3 building (the original minor platform) to the addition of cantilevered awnings in 1925. All platform buildings are of aesthetic significance as good representative examples of their types and periods. The surviving interior and exterior detailing of the 1884 platform buildings and awnings is considered rare on the Illawarra line.¹

Sydenham Pit and Drainage Pumping Station No. 1

The existing statement of significance for Sydenham Pit and Drainage Pumping Station No. 1 is as follows:

The Sydenham Pit and Pumping Station is of historic, aesthetic and technical significance. Historically, it is the first such infrastructure built in the SWC system and is an intact and major component of the Marrickville low level stormwater drainage infrastructure that was built in response to increasing urban expansion since the 1870s in an area prone to

¹ <http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=4801154>.

flooding. Its large scale and labour-intensive construction method of excavating the pit reflects the abundance of labour during the Great Depression and the type of public works undertaken to provide relief work for the unemployed.

Aesthetically, the use of pitched dry packed ashlar sandstone walls to line the sides of the pit provides a pleasantly textured and coloured finish to the pit. It is a major landmark and dramatic component of the industrial landscape of Sydenham particularly as viewed from the railway. The pumping station is a very good example of a utilitarian building displaying Inter-War Mediterranean style architectural details. Technically, the pumping plant contains good working examples of 1930s pumps, particularly three Metropolitan Vickers pumps, and its original electrical mains equipment has been preserved in situ during upgrading in c1992.²

2.3 Sydenham Metro Upgrade Project Description

The Sydenham Metro Upgrade Project forms part of the larger Sydney Metro City and Southwest project. The project comprises the following:

- The Sydney Metro City & Southwest project consists of a 30 km metro rail line, extending from the end of Sydney Metro Northwest at Chatswood under Sydney Harbour, through new central business district (CBD) stations and south west to Bankstown.
- The Sydenham portion of the works extend from the Bedwin Road Overbridge at St Peters in the north to Fraser Park, Marrickville, in the south.
- Works will include the demolition and reconstruction of platforms 1 and 2 at Sydenham Station for metro rail operations and a new aerial concourse connecting to new station entries at Railway Parade and Burrows Avenue. Upgrades to transport interchange facilities and provisions for active transport will made.
- Adjustments will be made to the Sydney Pit and Drainage Pumping Station—including a new aqueduct over the pit, new pumping station, and new maintenance access ramp.
- Ancillary infrastructure and works will be undertaken—including fencing, maintenance access, utilities works, drainage, noise barriers, road and transport network works, bridge works, and temporary facilities to support construction.³

² <http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=5053883>.

³ Chatswood to Sydenham—Sydenham Station and Sydney Metro Trains Facility South Modification Report, 2018, p. ii.

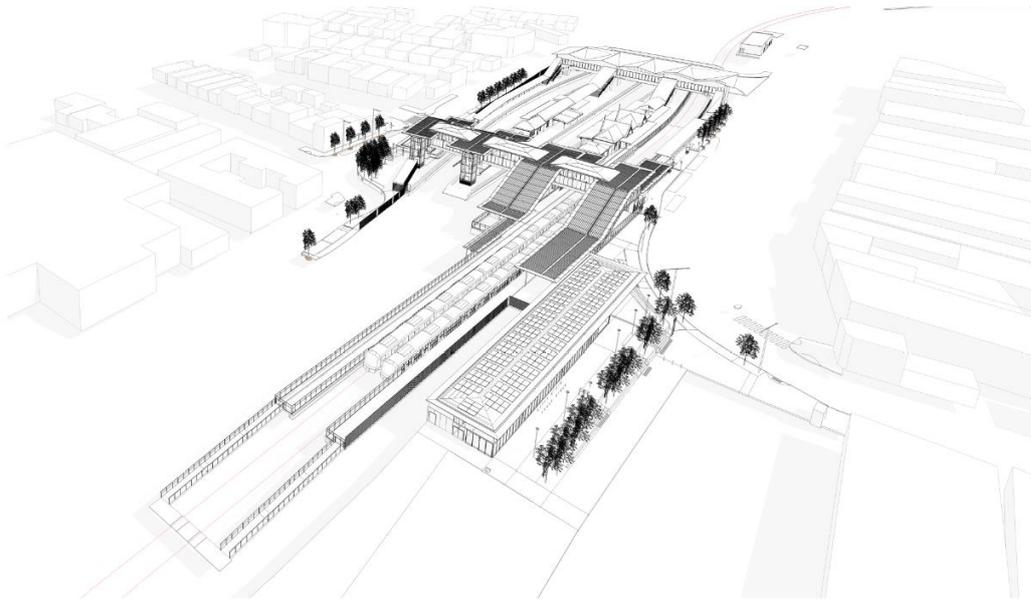


Figure 3. Aerial view of proposed site layout (HASSELL + Weston Williamson, Sydney Metro City & Southwest, Sydenham Metro Upgrade Project, Architecture General Arrangement—Isometric View—South, Sheet 1/1 A1).

2.4 Constraints

There are several constraints in scoping this HIP, including the following:

- A public art program is currently being developed in isolation from the interpretation plan. No information on the program is available at this stage.
- The detailed design of the project will be unknown until Stage 3 of the interpretation planning process. To allow for changes in the broader design scope, there needs to be some flexibility in the locations and design of devices outlined in this HIP.
- There is existing interpretation on the current concourse, which provides a text-heavy overview of the history of the railway station and line. The interpretation is not well considered with regards to its location within the concourse and low position on the wall.
- There is the potential for archaeological remains—both European and Aboriginal—to be uncovered during ground disturbing works across the site. Significant archaeological remains collected through the excavation program will need to be considered as part of the interpretation design. As this HIP has been prepared prior to the archaeological program, any significant archaeological remains will need to be assessed for inclusion during Stage 3 of the interpretation delivery. This may include a physical or digital interpretation solution, depending on the cultural material.
- There is limited capacity for the meaningful display of artefacts at the site.
- The following must be considered:

- ◆ Interpretation devices must not impede movement within the site, especially within high traffic areas such as the new concourse.
- ◆ Interpretation should avoid adverse physical or visual impacts to heritage fabric.
- ◆ Interpretation should avoid obscuring or blocking lines of sight to significant heritage fabric.

2.5 Existing Interpretation

Cataloguing existing interpretation within a site is key to understanding its interpretative potential. In the case of Sydenham Station, there are three interpretation panels existing within the current concourse and cover the following interpretive stories:

- ‘The Illawarra Railway’—the development of the Illawarra Railway line and construction of Sydenham Station.
- ‘Living and Working on the Railways’—station staff and an overview of the station masters house.
- ‘Development of the Station’—overview of major physical changes to the station.

Panel #	Text
1 (left)	<p>THE ILLAWARRA RAILWAY</p> <p><i>The Illawarra Railway was built in the 1880s to link Sydney with the coal and milk producing districts around Kiama and Wollongong. At the time Sydenham was also a rural area, occupied by market gardens, brickyards and dairies.</i></p> <p><i>Local land owners capitalised on the construction of the railway, fuelling a local property boom. Areas alongside the railway were quickly subdivided into small housing lots, suitable for working class families, but new home owners soon found out that the low-lying land was prone to flooding. Sydenham Road was originally known as the Swamp Road.</i></p> <p><i>Sydenham Station opened on 15 October 1884, on the first section of the Illawarra Railway between Redfern and Hurstville. It was originally names Marrickville Station because it was then the closest station to the village of Marrickville. In 1895 the Bankstown Railway was constructed and a second Marrickville station opened closer to the village. The first Marrickville station became the junction of the Illawarra and Bankstown lines and was renamed Sydenham.</i></p> <p><i>Initially, Sydenham Station had only two tracks and two side platforms. The platform buildings opened onto the neighbouring streets. The original buildings are still standing on Platforms 2/3 and 4/5, but are no longer accessible from street level. They are now preserved as good examples of Victorian era railway infrastructure, which demonstrate the high status of the railway within the community in the late 19th century.</i></p> <p><i>Although both buildings have been modified, for the expansion of the station and in response to changing travel customs, you can still see evidence of their original form and character. Original architectural elements include picturesque gables in the Gothic style and ornamental iron verandahs along the platforms.</i></p>
2 (middle)	<p>LIVING AND WORKING ON THE RAILWAYS</p> <p><i>Before the invention of automated signalling and computers the people employed at railway stations performed a wide range of manual work. This included collecting tickets,</i></p>

Panel #	Text
	<p><i>assisting with luggage, loading and unloading mail and goods from trains, operating the signals, and looking after the platforms, tracks and points.</i></p> <p><i>In the early days, stations were staffed almost exclusively by men, except for the refreshment rooms, which were run by women. Many men stayed with the railways for their whole working life, often starting out as junior porters and working their way up through the ranks to the position of signaller or station master.</i></p> <p><i>Most urban stations were managed by a station master, while gatekeepers were in charge of level crossings. The Railways Department often provided free housing for these men and their families.</i></p> <p><i>William Morse was one of the first station masters at Sydenham Station. He staying in the job for more than 20 years, from at least 1886 until 1906. William lived in a house especially built for the station master on the southeast side of the station, near Marrickville Road (now Railway Road). William and his wife Annie raised six children in the house, which had three bedrooms, a sitting room, a kitchen and scullery. The toilet was in a separate shed out the back. The house was in use until the early 21st century, but has now been demolished.</i></p>
3 (right)	<p>DEVELOPMENT OF THE STATION</p> <p><i>One of the first major changes to Sydenham Station was the construction of a footbridge in 1891, to make it safer for pedestrians to cross the tracks. In 1913 a new footbridge was installed, with a weatherboard booking office added to the top. In 1927 the NSW Railways Department opened a refreshment room in the booking office building. Railway refreshment rooms were located at most major stations and junction stations to sell food and drink to railway passengers. Tea and coffee, cakes, scones and meat pies, sit down meals, and even liquor were provided for railway passengers.</i></p> <p><i>In 1912–1913 the number of tracks passing through Sydenham Station was increased from two to four and the original side platforms were converted to island platforms. The Gleeson Road overbridge was built beside the footbridge and level crossings at Sydenham Road and Marrickville Road were closed. The houses of the gatekeepers, who managed the level crossings, were later demolished. In 1926 the Illawarra and Bankstown lines were electrified and new electric powered trains began to replace steam train services.</i></p> <p><i>The present side platforms were added to the station in 1925 (Platform 6) and 1962 (Platform 1). The overhead booking office was rebuilt in approximately 1986, and again in 2013. The 1913 footbridge was also replaced in 2013 by the present concourse, which was designed to provide lift access to the platforms.</i></p>

As per the ‘Sydenham Station and Junction Heritage Interpretation Strategy Reference Design’ [Document Reference NWRLSRT-PBA-WECHE-REP-000002], 5 April 2017, existing interpretation on site is described as follows:

The interpretative signage at Sydenham Station consists of three panels that include historical information and images. There is a lot of information and the panels are not well placed—approximately one metre of the ground, on the concourse, next to staircase to Platform 4/5. These panels are easily missed and also require any interested commuters to stoop or kneel on the concourse in order to read them. They would be better located at

eye level and in a more obvious location (possibly by the ticket gate) as the concourse is usually a busy thoroughfare and detailed interpretive media would be unnoticed or ignored.

While this interpretation provides important information about the place, social and cultural values of the place are not examined.



Figure 4. Existing interpretation within the concourse.



Figure 5. Detail view of panel 1 (left side).

3. Customer Analysis

The Heritage Interpretation Strategy identifies the demographic of the customers likely to use Sydenham Station. An extract is summarised below.

3.1 Census Data

Sydenham is a highly urbanised, residential and commercial suburb. The 2016 census counted 7,846 people living in the Sydenham, Tempe and St Peters, up 656 people from 2011, with 1,145 people specifically living in Sydenham. The median age for the suburb is 36. A total of 659 residents noted that they travel to work on the train.

With respect to employment, the work force will be aged from 15 and 60. They will have the most interaction with interpretation at the site, travelling in and out of the station in the morning and afternoon on a daily basis throughout the working week (Monday–Friday).

The 2013 station barrier counts for Sydenham Station are listed below.

Table 2. 2013 Sydenham Station barrier counts.

Time	In	Out
2 am–6 am	50	50
6 am–9.30 am	1,960	1,740
9.30 am–3 pm	1,400	890
3 pm–6.30 pm	1,740	1,790
6.30 pm–2 am	470	1,150
24 hours	5,620	5,620

Note: Sydenham Station ranked number 46 in the Sydney metropolitan area on the number of train station barrier counts.⁴

3.2 NSW Trainlink/Opal Card data

The NSW Trainlink statistics for Opal Card usage data along railway lines that pass through Sydenham Station have been outlined below. As a junction station, a significant portion of these customers are likely to switch trains at Sydenham.

Table 3. NSW Trainlink data

Line	2018 annual usage (to date)
T3 Bankstown Line	22,082K
T4 Eastern Suburbs and Illawarra Line	51,394K
T8 South Coast Line	7,448K

⁴ Train Statistics 2014, Appendix 2, p. 72.

4. Historical Overview

Environmental

Previous research from the EIS, Modification Report, Submissions Report and the addendum ARD indicates that the Sydenham Metro Upgrade Project works area is on the margins of the former Gumbramorra Swamp; and is located at the foot of the declining Hawkesbury Sandstone and Ashfield Shale ridges of the Marrickville area, in a relatively low-lying, narrow area surrounded by low spurs. The swamp itself has been drained, filled in and canalised since the 1890s, but prior to non-Aboriginal occupation the area was characterised by mudflats, mangroves and saltmarsh. The swamp was a tidal estuary that emptied into the Gumbramorra Creek and eventually into the Cooks River; it supported diverse and abundant wildlife, making it an ideal economic resource gathering area for local Aboriginal people. Those parts of the wider landscape that were slightly elevated above the floodplain of the swamp would have been ideal campsites and activity areas for local Aboriginal people.

No Aboriginal objects have been previously identified in the project area; and the closest recorded site is a Potential Archaeological Deposit (PAD) in Fraser Park, immediately west of the study area. Further investigation of this PAD by Susan McIntyre-Tamwoy in 2003 revealed that it was likely a naturally-occurring (i.e., not cultural) shell bed formed by fluvial processes, which had been partially destroyed through the installation of new underground electricity cables in 2009. On the whole, however, previous archaeological investigation of the area has been constrained to surface investigation only; as existing buildings and built-up environments obscure the ground surface and hinder inspection of the underlying soils.

Deep, Quaternary soils of the Birrong Soil Landscape are associated with the Gumbramorra Swamp and its margins, and may date back to the Pleistocene (more than 10,000 years before present). Sediment samples from boreholes in nearby Murray Street and Edgeware Road reveal that soils of the area comprise between 0.7 m and 1.3 m of modern fill and historic deposits, overlying natural silty clays, sandy peats and muds, to depths of 7.5 metres below ground surface. These results suggest that deep residual soils with potential to contain Aboriginal objects are present across the project area, even where historical use of the site has caused some ground surface disturbance.

Considerable ground surface disturbance has occurred as a result of the construction of the Sydenham Pit and Drainage Pumping Station and its associated concrete-lined drainage canals, while parts of the Sydenham Station line have cut into the surrounding landscape to the shale bedrock. In these discrete areas where significant disturbance has occurred, there remains a low likelihood of Aboriginal objects and intact Aboriginal deposits surviving.

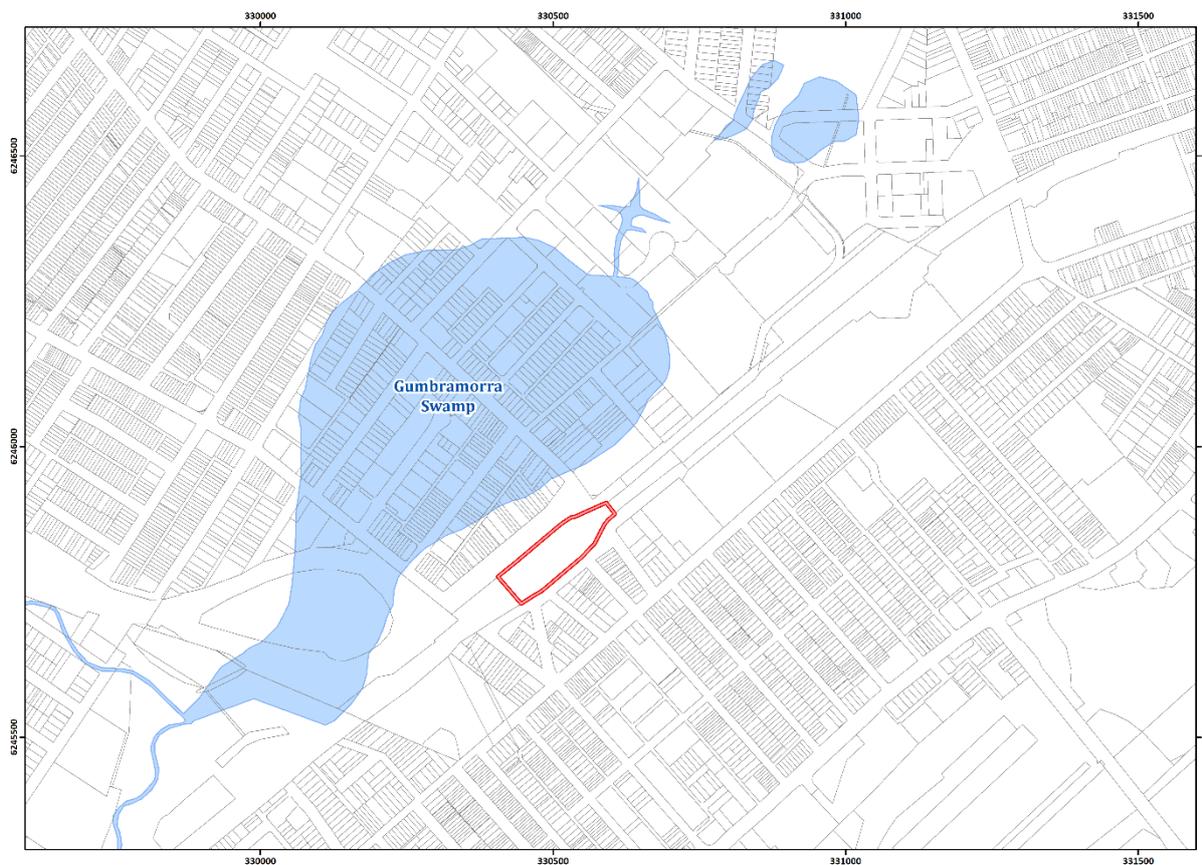


Figure 6 Gumbramorra Swamp shown on current cadastre (Extent Heritage, 2018).

European History Overview

Much of western Sydenham is located within Thomas Moore's Douglas Farm of 470 acres granted in 1799. A further grant of 700 acres was made in 1803 followed by purchases of adjoining land so that by 1807 held 1920 acres, making him one of the largest landowners in the Cooks River District. Douglas farm as the Sydenham property was known had extensive stands of timber. A small portion of the property was under cultivation, primarily maize and wheat. The eastern boundary of Moore's land was formed by the present line of Unwins Bridge Road. The whole of the study area south of a line extending westwards from the Mary Street/Unwins Bridge Road intersection lies within the former Douglas Farm. Moore's property was subsequently leased to Garnham Blaxcell although there is little evidence to indicate large-scale clearing or construction on the property. The farm was purchased by Dr Robert Wardell on 21 July, 1830 and renamed the Petersham Estate.⁵ The estate extended from Parramatta Road at Lewisham to Cooks River. Following Wardell's murder in 1834 the estate was divided

⁵ Cashman and Meader (1990): 40.

between his sisters Anne Fisher, Margaret Fraser and Jane Isabella Priddle.⁶ Sections of the property were sold progressively from 1834 onwards but the Sydenham section of the estate was not subdivided for sale until 1857 as the Sydenham Farms. These were 4-acre to 10-acre blocks. Up-take of the blocks was slow with few of the farmlets being occupied or built-on by 1881.

The northern portion of the study area crosses three other early land grants, those of John Fincham (30-acres), James Waine (30-acres) and Thomas Dukes (30-acres). No evidence has been located for the presence of farmhouses or other buildings on these properties within the study area. By 1857 Fincham's and Waine's farms had become the property of Thomas Smidmore, was a successful businessman and alderman on the Sydney City Council, from 1842 to 1850. The Sydenham property was named Silverleigh and became Smidmore's principal residence until his death in 1861. The residence fronted Unwins Bridge Road opposite Edith Street.

A significant change to the district was the construction of the Illawarra railway line from Eveleigh to Kiama. Work commenced in 1882 and the line as far as Hurstville was opened in 1884. The present station at Sydenham was constructed as Marrickville Station with platforms 2/3 and 4/5 being constructed in anticipation of a branch line to Bankstown. This latter line was constructed in 1895 and extended from Sydenham to Belmore. Road access across the lines consisted of level crossings in the north (Sydenham Road-Bailey Street) and in the south (Marrickville Road-Railway Road). A stationmaster's residence was also constructed at 117 Railway Road as part of the station complex. This unlisted structure was demolished by Railcorp between February and April 2014 with an intention to sell the property and citing contamination remediation as the reason for demolition.

Sydenham Station has undergone a number of major modifications since its opening in 1884. In 1925 platforms 1 and 6 were constructed although platforms 1 and 2 remained inactive until the early 1950s. The Gleeson Avenue concourse also underwent significant modification. The steel footbridge was replaced by a concourse attached to the Gleeson Avenue overbridge. The weatherboard ticket office burnt down in the 1980s. The replacement concourse was removed and replaced by the existing concourse in 2012 to 2013.

Railway buildings also occupied the area on the northern side of the Bankstown line west of Gleeson Avenue. These structures included a residence (removed by 1943) and a signal box on the southern side of the Marrickville Road level crossing.

The presence of the railway was a stimulus to development and a number of the former small-holdings were subdivided into residential blocks. The floods of May 1889 did, however, illustrate the problems associated with attempting to build on a former swamp. The Gumbramorra Swamp

⁶ Cashman and Meader (1990): 88.

was restricted to a single creek-line flowing into Cooks River and the surrounding lands partly filled. In 1898 construction of a network of formal low-level drainage channels in Marrickville commenced. This initial program of works was followed by a second stage in 1903. The resulting network of channels and culverts discharged into Cooks River immediately west of Tempe Railway Station. Later improvements to the scheme between 1935 and 1941 consisted of the construction of the Sydenham Drainage Pit that discharged by means of a pumping station into the existing channels. In 1965 much of the channel network west of the rail corridor was widened to its current dimensions.

The creation of a large area of flat land stimulated the development of industries within the Sydenham area in the period between 1895 and 1920. These included the Vicars Woollen Mill, Sydenham Pottery Company, Fowler Potteries, Sydney Steel Company and Jubilee (later Sydney) Brickworks and Marrickville Margarine Company. The Sydney Steel Company is located immediately adjacent to the work zone and lies within the Sydney Metro Trains Facility Area.

Messrs Ramsay and Johnston established a small pottery in Garden Street as early as 1907 as the Sydenham Pottery Works. In 1909 Alfred Dawes, son of Naasson Dawes, General Manager of Bakewell Brothers brickworks section, provided financial backing for Ramsay and Johnston with the company operating under the name A. Dawes & Co. Following Dawes transfer to R. Fowler's as General Manager of their brickworks section in 1910 the pottery operated under the name Ramsay and Johnston. The pottery may have operated as the Sydenham Pottery Company in 1916 following acquisition of the firm by Thomas Arthur Ashton, Wilfred Cox and William Bloomer. Thomas Arthur Ashton (1870 Longton, Staffordshire, England—1957 Redcliffe, Queensland), was a porcelain decorator from Staffordshire. The partnership was dissolved in 1924 and in the following year R. Fowler Ltd, located on the adjoining block to the east, purchased the Sydenham Pottery Company. Although Fowler's absorbed the Sydenham Pottery Company it continued manufacturing under its own name until at least 1947. The precise range of wares produced is unclear. The earliest material appears to have been restricted to bottles. Later wares were primarily domestic vessels such as toilet sets, mixing bowls and art pottery. The date at which the pottery ceased operations is unknown. Fowler's Pottery complex ceased operation in Marrickville in 1975.

Between 1916 and 1925 a goods line referred to as the Sydenham to Botany rail line was constructed. At the Sydenham end of the line significant earthworks and embankments faced in brick were constructed along Marrickville Road and Railway Parade providing elevated road access across the rail line at Gleeson Avenue. Following completion of the overbridge the level-crossings at Sydenham and Marrickville Roads were closed.

The following figures provide an overview of the history of the areas as per the above.



Figure 7. Platform 2/3 at right 1907 (Marrickville Image Library rhpc005).



Figure 8. New South Wales Government Railways C32-Class Locomotive 3267, Sydenham Station, [n.d.] View from former concourse above Platform 5 looking across Platform 6 (Australian Railway Historical Society, New South Wales Division).



Figure 9. Sydenham- Botany Goods Line—Sydenham (Illawarra Line) underbridge (undated) looking south (Pollard (1988).



Figure 10. Construction of retaining wall adjacent to Railway parade, Sydenham n.d. (1916)
Looking northwest, the image shows the now-concealed rear of the retaining wall (SLNSW image 221584).



Figure 11. Sydney Steel Company, 1917. View east to railway line showing cutting in front of 'Silverleigh' (Marrickville Library Asset 003152).



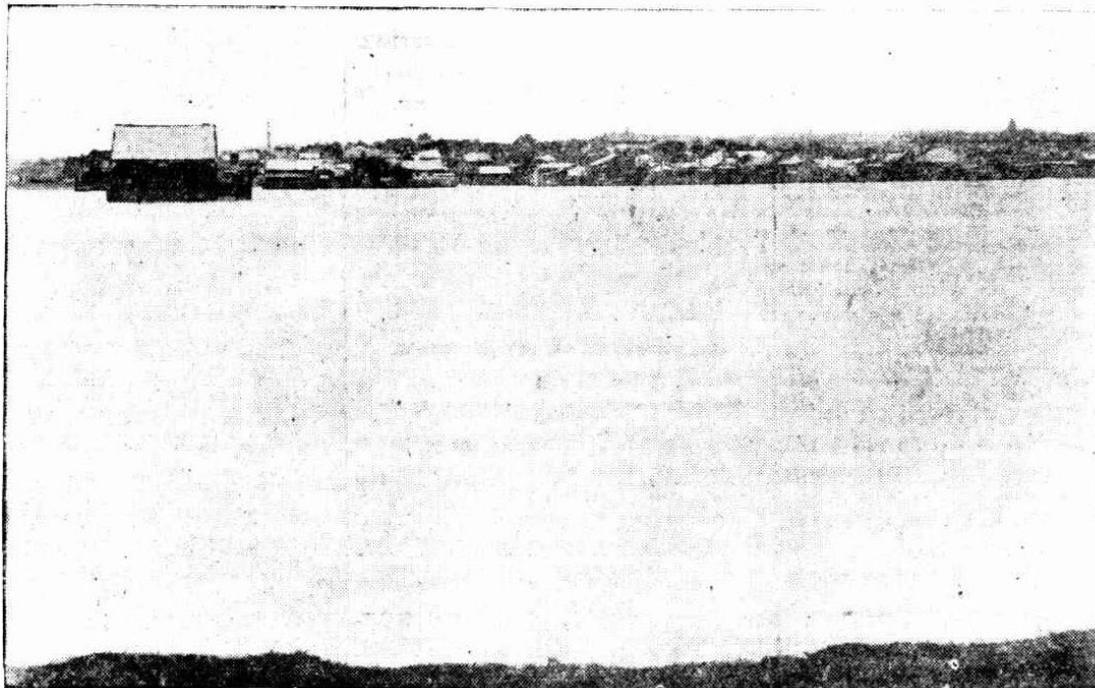
Figure 12. Sydney Steel Company, 1917. View northwest to Edinburgh Road (Warwick Stuart, Sydney Steel: An Illustrated History of The Sydney Steel Company 1910–1979 [2012]).



Figure 13. Sydenham Pottery Company, c.1948. View southwest detention it in foreground to Edinburgh Road (Warwick Stuart, Sydney Steel: An Illustrated History of The Sydney Steel Company 1910–1979 [2012]).



Figure 14. Flood of 1889 (Illustrated Sydney News, 6 June 1889, p 14).



LOOKING TOWARDS PETERSHAM—A BIG SHEET OF WATER.

Figure 15. Flood of 1905, view looking northwest from Sydenham (Evening News 5 Apr 1905, Page 2).



Figure 16. Bridge, stormwater channel, Sydenham–Botany rail, 3 November 1916. View shows the 1898 stormwater channel, the Marrickville Road railway embankment wall (left), the Sydenham station concourse buildings (top right) and the buildings that occupied the area adjacent to the Bankstown line between Gleeson Avenue and Marrickville Road (SLNSW image 221599).



Figure 17 Globe Worsted Mills, Barclay Street, Marrickville. View from Marrickville Road during the flood of July 1931. This has been included to exemplify the water management issue in Marrickville. It is not specifically relevant to the subject site (Source: Marrickville Image Library Globe Woollen Mills, 1930 flood Asset 003142).



Figure 18. Works at Sydenham 1935. Excavation of the Sydenham Drainage Pit looking northwest towards the Garden Street/Shirlow Street intersection. The Sydenham Pottery Company at top right (SLNSW 81937).



Figure 19. Sydenham Pit and Drainage Pumping Station No.1 1948 (Sydney Water).



Figure 20. Storm drain, Sydenham 1965. The image shows the removal and replacement of the c.1898 brick channel by the extant concrete channel adjacent to Sydney Steel Company, looking north (SLNSW Government Printing Office 2—26989).

Sydenham Station

Sydenham Station was built on a duplicated line from Illawarra Junction to Hurstville and opened in 1884. The western platform contained a major third-class brick station building having a detached toilet block at each end separated by walled courtyards while the eastern platform contained a large second-class brick station building. The station opened as Marrickville but it obtained its present name in 1895 with the opening of the Belmore branch line. In 1907 the platforms were extended.

The impressive station was obviously intended to serve the Marrickville township proper, but it was distant, surrounded by industrial and rural estates and only grew as a station by reason of the need to cope with the branch line junction. In 1907 the line from Edgeware Road to Sydenham was quadruplicated to serve the Belmore to Bankstown extension when it opened in 1909. This resulted in confining both buildings on island platforms so that passengers had to reach the platforms by an extended footbridge. A new timber overhead booking office on a steel support frame was built between Platforms 3 and 4 and steel footbridges were eventually extended to all platforms c. 1914.

The 1914 overhead footbridge at Sydenham was a haunched beam design that consisted of tapered cantilevers resting on platform trestles and supporting shallow beams over the railway tracks where headroom over rolling stock can be critical. The footbridge was manufactured by Dorman Long & Co. Ltd. Middlesbrough England (stamped on posts), who also engineered the Sydney Harbour Bridge.⁷ Thirty sets of such footbridges were built from 1909 to 1935, twenty-eight in the Sydney metropolitan area.

To provide for the proposed Eastern Suburbs Railway, two additional tracks were put in so that in 1925 the brick standard island platform building on Platform 6 was built. In 1926 the lines were electrified at Sydenham. Soon after, in 1927 the refreshment room was opened for factory workers in the area. As the additional tracks were never utilised for the Eastern Suburbs Railway they have been mainly used for the Bankstown line trains. In 1963 a brick parcels office building was constructed on Platform 1 but closed in the late 1980s.

The weatherboard ticket office on the overhead footbridge burnt down in the mid-1980s. In the late 1980s a new brick overhead booking office and a new metal-clad shop were built on the existing c.1914 footbridge structure, and new canopies built over the stairs and connected to platform buildings.

The group currently includes all of the brick platform buildings and their awnings, the brick faced platforms, the steel footbridge structure and stairs.

⁷ <http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=4801154>.

An upgrade was undertaken c2012 including new concourse and station building, new lifts, new canopy, and replacement of 1980s OHBO and footbridge. The OHBO was a brick flat roofed shop and ticket office with aluminium framed glazing, and roof overhang to shelter entry from Gleeson Avenue.

Sydenham Pit and Pumping Station

With the completion of the Illawarra railway beyond Sydenham in the 1880s, the urbanisation of the Marrickville Valley increased rapidly. It was soon found that the valley had significant drainage problems, which were partly solved by the construction of a stormwater pumping station in Carrington Road, Marrickville in 1897 (now known as Marrickville Sewage & Stormwater pumping Station—SP271) and three main stormwater channels, comprising the Eastern, Western and Central Channels. In the 1930s the government decided to improve the drainage system, which included an allocation of unemployment relief funds for drainage works in Marrickville Municipality. The scheme included the drainage of the northern section of the low-level area north of Marrickville Road, comprising the excavation of a storage pit, the erection of a pumping station with a rising main discharging into the Eastern Channel and the construction of a system of channels discharging into the pit. The pit and pumping station were constructed by the Public Works Department in the late 1930s and transferred to the MWS & DB in 1941.⁹

Sydenham (Illawarra Line) Underbridge

A goods line from Marrickville to the industrial area at Botany was planned c1914, as an extension of the Metropolitan Goods Lines. Work commenced in 1916 with the construction of a number of cuttings and low-level embankments. For the last high-level section from Sydenham to Marrickville, the embankment was formed by a method commonly used in the USA. A temporary timber trestle viaduct was built such that the ballast trains from Botany could tip the sandy material through the open transom deck to gradually build up the embankment. Eventually the temporary trestle viaduct was filled over and abandoned. The Botany Line was opened on 11 October 1925.

Pratt Trusses were introduced to Australia from the U.S in 1892 with the construction of the light-rail Yass Tramway. Thereafter they became the standard for Main Line railways for spans over 30 metres. While previous forms of truss had lent themselves to construction from timber, with stocky timber sections with good compressive and buckling resistance forming the diagonal members, the Pratt Truss reversed the direction of load in the diagonal members, enabling light rods or flat bars to be used in tension, making steel trusses highly efficient. The New South

⁹ <http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=5053883>.

Wales railways continued to employ the use of steel Pratt trusses for major bridge crossings until the advent of reinforced and prestressed concrete in the 1970s¹⁰.

¹⁰ <http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=4805746>.

Table 4. Description of key themes.

Theme	Description	Relation to site
Aboriginal cultures and interactions with other cultures	Activities associated with maintaining, developing, experiencing and remembering Aboriginal cultural identities and practices, past and present; with demonstrating distinctive walks of life; and with interactions demonstrating race relations.	Gadigal people of the Eora nation Use of the Gumbramorra Swamp Part of the Bulanaming district
Towns, suburbs and villages	Activities associated with creating planning and managing urban functions, landscapes and lifestyles in towns, suburbs and villages	Thomas Moore's 1799 grant Small-scale residential subdivision—failed subdivisions in the 1880s and 1890s Coping with floods Local Industry
Utilities	Activities associated with the provision of services, especially on a communal basis	Sydenham Pit and Drainage Pumping Station within the wider drainage network
Industry	Activities associated with the manufacture, production and distribution of goods	Timber-getting Sydenham Pottery Company Sydney Steel Company Twentieth-century industrial growth Migration post-WWII
Transport	Activities associated with the moving of people and goods from one place to another, and systems for the provision of such movements	Transport hub—bus, tram, train Junction Station Road access development (Gleason Avenue embankments) Goods transport (Sydenham–Botany Line)

6. Consultation

The Minister's Conditions of Approval (SSI 7400 MOD 4) require that the HIP be prepared for the Project in consultation with the Heritage Council of NSW, the relevant Council and Registered Aboriginal Parties.

Consultation with the Aboriginal Community was undertaken during concept design as part of the Sydney Metro Chatswood to Sydenham Environmental Impact Statement (EIS) and also during preparation of the Aboriginal Cultural Heritage Assessment Report (ACHAR), in accordance with OEH's guidelines Aboriginal cultural heritage consultation requirements for proponents 2010. This process identified 21 organisations and/or individuals who registered an interest in the project:

- ◆ Metropolitan Local Aboriginal Land Council;
- ◆ Gandangara Local Aboriginal Land Council;
- ◆ Darug Land Observations (Gordon Workman);
- ◆ Darug Land Observations (Jamie Workman);
- ◆ Tocomwall;
- ◆ Darug Aboriginal Cultural Heritage Assessments;
- ◆ Kamilaroi Yankuntjatjara Working Group;
- ◆ Woronora Plateau Gundangara Elders Council;
- ◆ Murra Bidgee Mullangari Aboriginal Corporation;
- ◆ Aboriginal Archaeology Service Incorporated (Tony Williams);
- ◆ Aboriginal Archaeology Service Incorporated (Andrew Williams);
- ◆ Gundungurra Tribal Technical Services (David Bell);
- ◆ Gundungurra Tribal Technical Services (Peter Forster);
- ◆ Gundungurra Tribal Technical Services (Christopher Payne);
- ◆ Bilinga Cultural Heritage Technical Services;
- ◆ Gunyuu Cultural Heritage Technical Services;
- ◆ Munyunga Cultural Heritage Technical Services;
- ◆ Murrumbil Cultural Heritage Technical Services;
- ◆ Wingikara Cultural Heritage Technical Services;
- ◆ DJMD Consultancy; and
- ◆ Aboriginal Heritage Office.

In accordance with the project MCoAs, the draft Heritage Interpretation Plan (V2) was provided to the above Registered Aboriginal Parties for their feedback and review. The review period spanned from the period 14 November - 12 December 2018, and a follow-up reminder of the end of report review period was distributed to all RAPs on 10 December 2018.

A complete log of actions and correspondence regarding Aboriginal community consultation (including any feedback received from the RAPs in relation to the report) is included in

Appendix 2. Where relevant, such feedback has been integrated into the report, and is summarised below:

- ◆ Kamilaroi-Yankuntjatjara Working Group (KYWG) provided a brief verbal and written response in support of the interpretation plan, its content and intent, but was unable to offer any additional information or advice.
- ◆ KYWG suggested that the original inhabitants of the broader Redfern area (which, they believed also extended to the Sydenham locale) had been displaced in the historic period.
- ◆ Aboriginal Archaeology Service Incorporated (AAS) provided a brief verbal response in support of the interpretation plan, its contents and intent. AAS was also unable to offer any additional information or advice, and recommended that the proponent liaise with the Metropolitan Local Aboriginal Land Council.
- ◆ No other feedback or comments were provided by the RAPs throughout the report review period.

The Heritage Interpretation Strategy and Heritage Interpretation Plan were presented to the Metro Heritage Working Group 6 August 2018 and 5 November 2018 respectively. The final reports were formally circulated to attendees of this forum following presentation. The HWG includes representation from OEH, amongst others. There were no requested actions arising from this consultation.

Inner West Council (IWC) sought feedback through the consultation process 28 November 2018. Extent Heritage provided clarification to IWC comments through correspondence 11 December 2018. IWC subsequently confirmed that this response satisfied their concerns, thereby closing out consultation with IWC.

7. Proposed Interpretation Devices

The following section describes the interpretive devices that will be included within the study area. Each device has been identified with an item number (in yellow). The following section includes an overall description of the device, its proposed location, limitations, relevant theme, example text (where relevant) and example visual media (where relevant).

Note: As the design is subject to change and there is a public art program being prepared in isolation to interpretation elements, all interpretation locations must be flexible in design and specific location.

Refer to Appendix 1 for the Heritage Interpretation Design Media package prepared by HASSEL + Weston Williamson.

HIP MEDIA - PROPOSED SUMMARY

14 PROPOSED SITES

Total 23 no. media, including:

- 3 no. sandstone plinth (1500w x 600d x 600mm h) and aluminium graphic panel (A2 420 x 594mm)
- 4 no. screen etching on 1200w x 2400h and 1200m aluminium cladding panel
- 1 no. screen etching on 1200w x 1700h m aluminium cladding panel
- 8 no. glazed screen etch film (3M) or similar 1200w x 1700h
- 6 no. inlay on 600w x 600d concrete seating
- 1 no. restoration of paint work

Item number below refers to Heritage Interpretation Report.

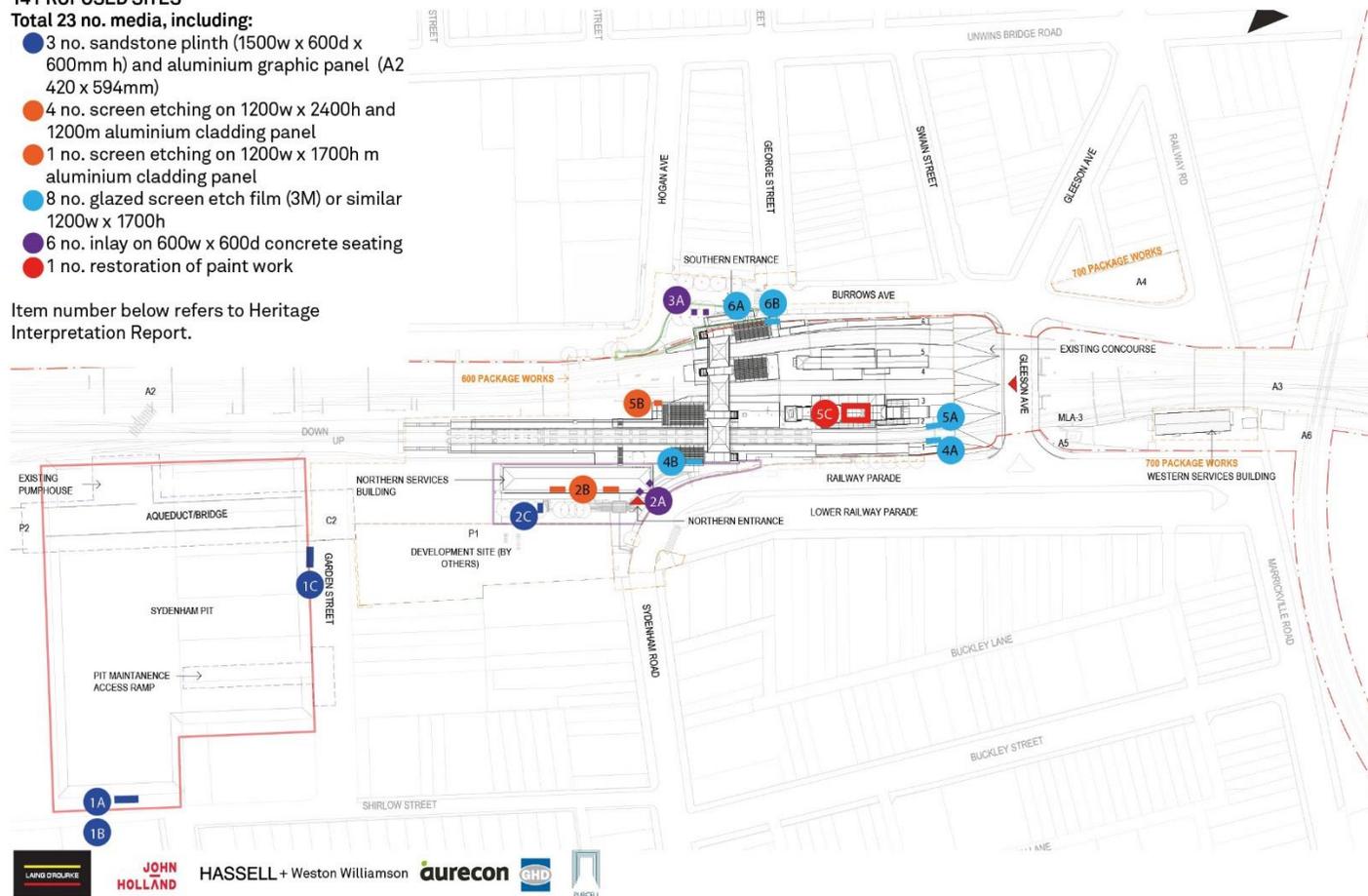


Figure 22. Interpretation device locations for Sydenham Station and Sydenham Pit and Drainage Pumping Station (Plan sourced from HASSELL + Weston Williamson).

7.1 Sydenham Pit and Drainage Pumping Station

It is proposed that a section of the requisite fencing to the perimeter of the Sydenham Pit and Drainage Pumping Station ('Pump and Pit') be dedicated to interpretation signage. This will be located along Shirlow Street where a clear outlook on the Pump and Pit is available, in addition to a potential location along Garden Street (subject to a future private redevelopment). Interpretation should explore the history and significance of the Pump and Pit as a utilities site and its role in water management for the area.

At Shirlow Street, there is also an opportunity to interpret activities associated with the manufacture, production and distribution of goods from Sydney Steel Company located north of the Pump and Pit, and the Sydenham Pottery Company. Interpretation at the Sydney Steel site itself is limited to the street that has low pedestrian traffic.

The signage panels will be designed to meet the performance and specification requirements of the SWTCs with regard to access and security, and be graphically designed with text, archival plans and photographs relevant to the identified themes and activities.

There may be opportunities for a Guided Tour of the Pump and Pit to explain first-hand the history and significance of the site.

Recommendation 1A

SIGNAGE PANEL X 1 (construction implementation opportunity)	
Description	Utilising an area which includes a vista over the Pump and Pit, a signage panel should be provided on the fence which includes text and visual content on the history and significance of the Pump and Pit.
Location	Shirlow Street—lookout over the Pump and Pit
Theme	Utilities
Specification	<ul style="list-style-type: none"> ▪ 1 x A2 panel (420 x 594mm) in etched anodised aluminium (colour) ▪ Mounted on sandstone plinth (1500w x 600d x 600h) ▪ Text and visual content ▪ Graphic layout to be drafted during Stage 3
Limitations	<ul style="list-style-type: none"> ▪ Include a short summary of the site only ▪ Low foot traffic expected
Example text	‘Sydenham Pit & Drainage Pumping Station 1’

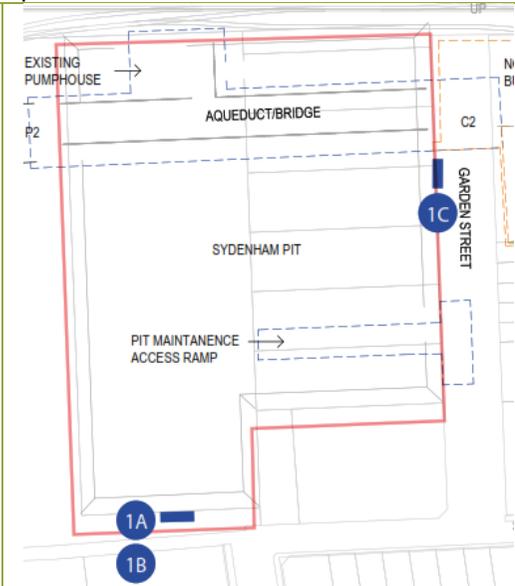


Figure 23. Location of device 1A.

Following the construction of Sydenham Station in the 1880s attempts were made to form a residential suburb between the station and Marrickville to the west. Three main stormwater channels, comprising the Eastern, Western and Central Channels were constructed in association with pumping station in Carrington Road, Marrickville. Further flooding of the district in 1905 prompted calls for a more efficient system of flood protection. A period of filling across much of the low-lying ground raised ground levels but it was not until 1935 that the drainage system was modified with the construction of the detention basin, pumping station and rising main. The construction was undertaken by the Public Works Department with labour paid for in part by an allocation of unemployment relief funds. The site was transferred to the MWS&DB in 1941. Subsequent modifications include repairs in the 1950s to sections of the stonework that had collapsed. In 1965 the Eastern Channel was reconstructed as dual, concrete channels and in 1968 the concrete floor of the pit and a central silt pit were constructed.

**Example
 visual
 media**



Figure 24. Works at Sydenham 1935. Excavation of the Sydenham Drainage Pit looking northwest towards the Garden Street/Shirlow Street intersection. The Sydenham Pottery Company at top right (SLNSW 81937).

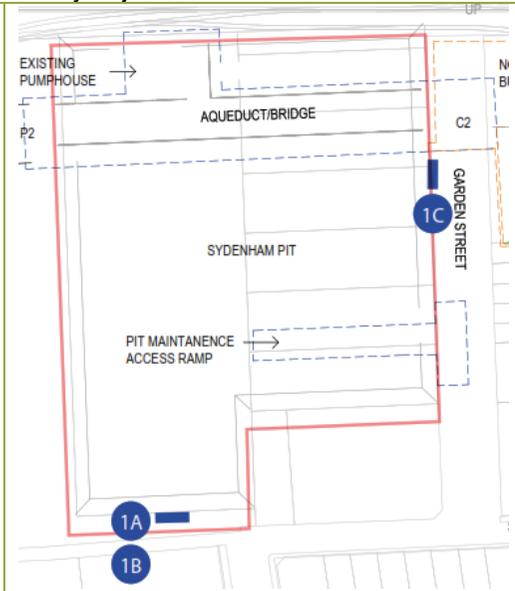


Figure 25. Storm drain, Sydenham 1965. The image shows the removal and replacement of the c.1898 brick channel by the extant concrete channel adjacent to Sydney Steel Company, looking north (SLNSW Government Printing Office 2—26989).



Figure 26. Sydenham Pit and Drainage Pumping Station No.1 1948 (Sydney Water).

Recommendation 1B

SIGNAGE PANEL X 1 (construction implementation opportunity)	
Description	<p>Utilising an area which includes a vista over the Pump and Pit, a signage panel should be provided on the fence which includes text and visual content on the history and significance of the Sydney Steel Company (Marrickville Dive Site).</p> <p>Interpretation at the Sydney Steel site itself is limited to the street, which is expected to have low pedestrian traffic. As a result, it has been included with interpretative elements nearby at the Pump and Pit to ensure the device has a larger audience.</p> <p>Interpretation of the Sydenham Pottery Company that was located immediately adjacent to this location.</p>
Location	<p>Shirlow Street—lookout over the Pump and Pit</p> <div style="text-align: right; margin-top: 10px;">  </div> <p style="text-align: right; margin-top: 5px;">Figure 27. Location of device 1B.</p>
Theme	Industry
Specification	<ul style="list-style-type: none"> ▪ 1 x A2 panel (420 x 594mm) in etched anodised aluminium (colour) ▪ Mounted on sandstone plinth (1500w x 600d x 600h) ▪ Text and visual content ▪ Graphic layout to be drafted during Stage 3

<p>Limitations</p>	<ul style="list-style-type: none"> There is no opportunity for interpretation at the former Sydney Steel Company site itself (Marrickville Dive Site).
<p>Example text</p>	<p>‘Sydney Steel Company’</p> <p>The Sydney Steel Company was established in 1909 to supply fabricated steelwork for the building industry. The absence of a local fabricator constrained local builders and architects who sought to follow the use of steel and concrete that had revolutionised architecture in North America. Steel fabricated at the Marrickville workshop changed the Sydney skyline. Notable buildings using steel from the Marrickville site include Farmers (now Myer) and David Jones department stores, the AWA Building (York Street), the AMP Building (Circular Quay) and the Wentworth Hotel (Phillip Street). The company was formed by Alexander Stuart a building contractor, mayor of St Peters, director of NSW Brick Company and chairman of the NSW Brick Masters’ Association. The company operated from the Marrickville site until 1975.</p> <p>‘Sydenham Pottery Company’</p> <p>The Sydenham Pottery Company was established by Messrs Ramsay and Johnston as early as 1907 in Garden Street as the Sydenham Pottery Works. The pottery may have operated as the Sydenham Pottery Company in 1916 following acquisition by potters Thomas Arthur Ashton, Wilfred Cox and William Bloomer. In 1925 R. Fowler Ltd, purchased the works and although Fowler’s absorbed the Sydenham Pottery Company it continued manufacturing under its own name until at least 1947. The company produced white and decorated wares, primarily kitchenwares and tablewares.</p>
<p>Example visual media</p>	<div style="display: flex; justify-content: space-around;"> <div data-bbox="499 826 1149 1329"> </div> <div data-bbox="1205 802 1843 1292"> </div> </div>

Figure 28. Sydney Steel Company, 1917. View east to railway line showing cutting in front of 'Silverleigh' (Marrickville Library Asset 003152).

Illustrated History of The Sydney Steel Company 1910–1979 [2012]).



Figure 30. Sydenham Pottery Company, 1935.

Recommendation 1C

GARDEN STREET (future opportunity)	
Description	There are currently no specific plans for Garden Street but considering the area is likely to become a lively residential area in the future, this street should be considered for interpretation opportunities in the future.
Location	Garden Street—Sydenham Pit and Drainage Pumping Station
Theme	Industry
Specification	N/A
Limitations	N/A
Example text	N/A
Example visual media	N/A

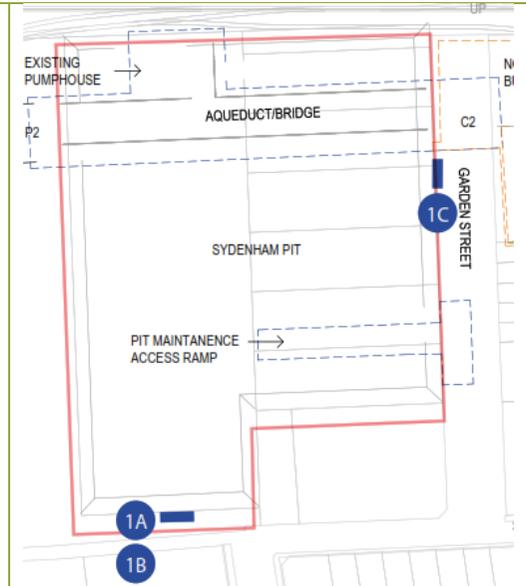


Figure 31. Location of device 1C.

Recommendation 1D

GUIDED TOUR (future opportunity)	
Description	There is potential to run a guided tour of the Pump and Pit (either with water or de-watered) to explain firsthand the history and significance of the site. For example, this could be undertaken as an annual open day, or included as part of the Australian Heritage Week— https://www.nationaltrust.org.au/ahf/ .
Location	Sydenham Pit and Drainage Pumping Station
Theme	Industry
Specification	N/A
Limitations	<ul style="list-style-type: none"> ▪ Safety risks associated with site access. ▪ Resourcing and advertising a guided tour. ▪ There are likely to be limited opportunities throughout the year to run tours. ▪ Protection of movable heritage items and heritage fabric, should it be made accessible to the public.
Example text	N/A
Example visual media	N/A

7.2 North Plaza

It is proposed that interpretation in the north plaza would integrate with landscape elements and features in locations where people are expected to dwell and potentially engage with interpretative media. Given the location of this area in the vicinity the Pump and Pit and the former Gumbramorra Swamp, there are several interpretation devices that can be included in this area. All of the devices will relate, in some way, to the history of water in the area.

Further consideration should be given to Aboriginal cultural heritage interpretation opportunities in this area during and after consultation with relevant RAPs.

Recommendation 2A

SEATING (construction implementation opportunity)	
Description	Seating, to be provided in the North Plaza, should be utilised for interpretation of the Aboriginal cultural values of the area. This will be based around a subtle integration of words with the furniture—areas where people are likely to dwell.
Location	North Plaza
Theme	Cadigal people of the Eora Nation and Gumbramorra Swamp
Specification	<ul style="list-style-type: none"> Concrete seating 600w x 600d module x 500(h) Etched words in concrete
Limitations	<ul style="list-style-type: none"> Must integrate with material palette for seating

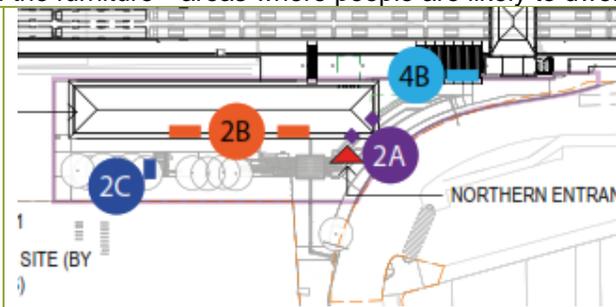


Figure 32. Location of device 2A adjacent to Sydenham Road.

	<ul style="list-style-type: none"> ▪ Consultation with registered RAPs must be undertaken prior to adopting Aboriginal interpretation.
<p>Example text</p>	<p>Example words include:</p> <ul style="list-style-type: none"> ▪ Cadigal—the local clan ▪ Gumbramorra Swamp—the former swamp directly north of the site ▪ Banga'ly—swamp mahogany (the tree that dominated the swamp) ▪ Bulanaming—name applied to the district up until the 1830s <p>Confirm appropriate words through RAP group consultation.</p>
<p>Example visual media</p>	<p>Motif—swamp mahogany</p>  <p>Figure 33. Swamp Mahogany (<i>Eucalyptus robusta</i>) (State Library of Victoria).</p>

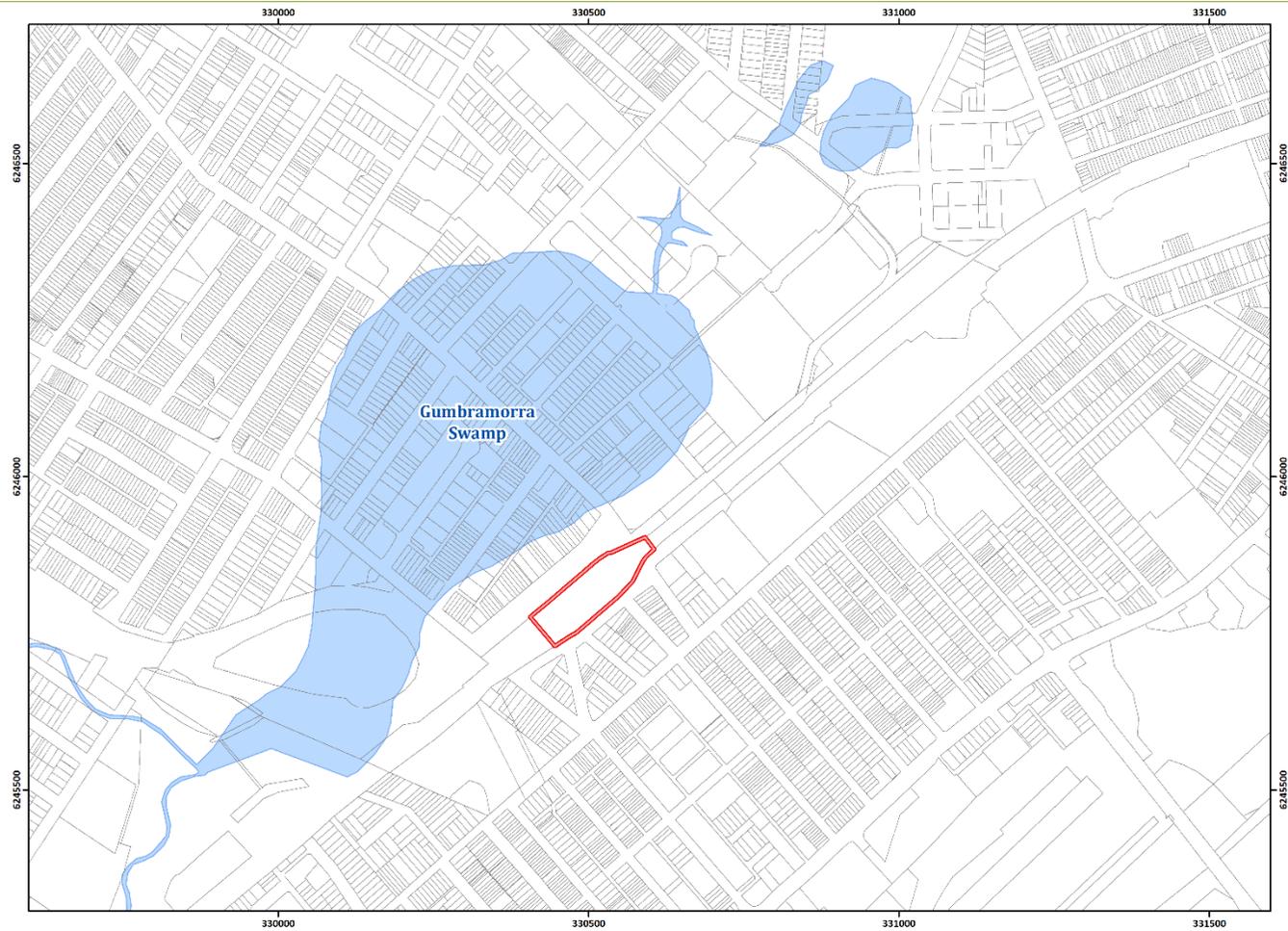
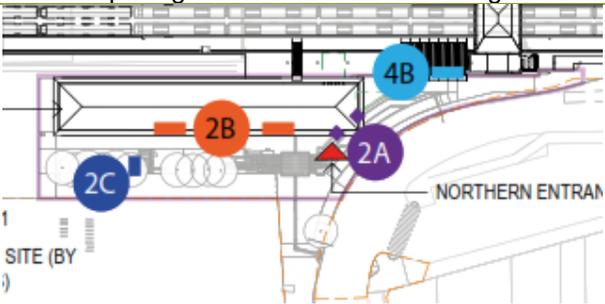


Figure 34. Map of the Gumbramorra Swamp (Extent Heritage, 2018).

Recommendation 2B

WALL SCREEN (construction implementation opportunity)	
Description	One the bike shed facing the North Plaza green space, include a wall screen depicting a scene of the area during flood.
Location	<p>North Plaza—northern elevation of the bike store.</p>  <p>Figure 35. Location of device 2B.</p>
Theme	Utilities, Towns Suburbs and Villages and water management
Specification	<ul style="list-style-type: none"> 4 x screen etching on 1200w x 2400h and 1200m aluminium cladding panel
Limitations	<ul style="list-style-type: none"> Little opportunity for text content
Example text	N/A—no text.
Example visual media	<div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">  <p>Figure 36. Flooding along Sydenham Road in the 1890s with Sydenham Station in the</p> </div> <div style="text-align: center;">  <p>Figure 37. Garden Street in flood (Australian Town and Country Journal 8 June 1889 p 27).</p> </div> </div>

background (Marrickville Image Library 003929).

Recommendation 2C

SIGNAGE PANEL (construction implementation opportunity)	
Description	A signage panel should be provided within the North Plaza green space which includes text and visual content on the history of water management in the area and significance of the Pump and Pit in relation to this history. The Pump and Pit and a historic culvert in the vicinity should also be addressed.
Location	North Plaza—exact location yet to be defined.
Theme	Utilities and water management
Specification	<ul style="list-style-type: none"> ▪ 1 x A2 panel (420 x 594mm) in etched anodised aluminium (colour) ▪ Mounted on sandstone plinth (1500w x 600d x 600h) ▪ Text and visual content ▪ Graphic layout to be drafted during Stage 3
Limitations	<ul style="list-style-type: none"> ▪ Include a short summary of the site only
Example text	Water management has been an important factor influencing life within this district. To the Cadigal the Gumbramorra swamp was a source of food and raw materials. To the first wave of European settlers it was a source of timber. Subsequent residential development was hampered by flooding, particularly in 1889, 1895 and 1897. The construction of three open channels and a pumping station in 1897 attempted to alleviate the situation. Subsequent floods in 1905 and then in 1931 resulted in the construction of the Sydenham Pump and pit and the reconfiguration of the open channels.

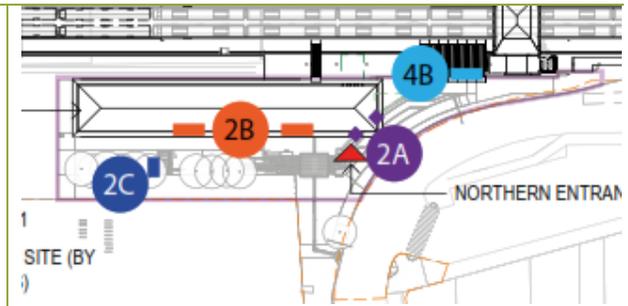


Figure 38. Location of device 2C.

**Example
visual media**

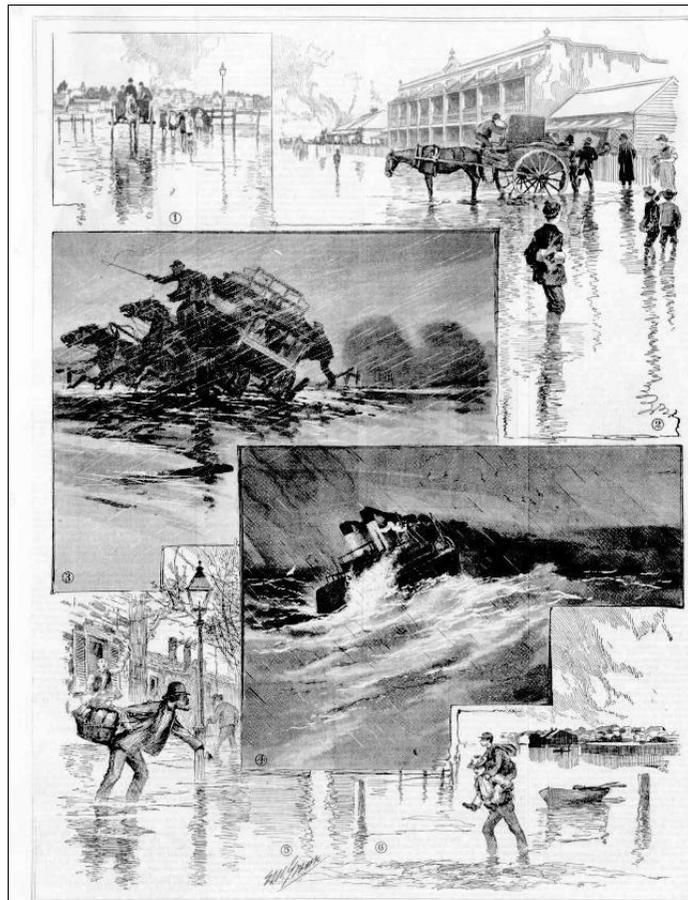


Figure 39. Newspaper reporting of the flood of 1889.



Figure 40. Eastern Channel looking north from Richardson Crescent towards Sydenham, 1899 (Marrickville Image Library 003941).



Figure 41. Storm drain, Sydenham 1965. The image shows the removal and replacement of the c.1898 brick channel by the extant concrete channel adjacent to Sydney Steel Company, looking north (SLNSW Government Printing Office 2—26989)

7.3 South Plaza

It is proposed that the south plaza provide interpretation of the precinct, context and setting of the station. Principally the proposal would be to engage with landscape elements and features in locations where people are expected to dwell and potentially engage with interpretative media.

In addition, there are opportunities to communicate activities associated with creating, planning and managing urban functions, landscapes and lifestyles in towns, suburbs and villages.

Recommendation 3A

SEATING (construction implementation opportunity)	
Description	Seating, to be provided in the South Plaza, should be utilised for interpretation of the European development of the area and the community. This will be based around the integration of words with the furniture - areas where people are likely to dwell.
Location	South Plaza—throughout
Theme	Towns Suburbs and Villages
Specification	<ul style="list-style-type: none"> ▪ Concrete seating ▪ 600w x 600d module x 500(h) ▪ Etched words in concrete
Limitations	<ul style="list-style-type: none"> ▪ Must integrate with material palette for seating.
Example text	Much of western Sydenham is located within Thomas Moore’s ‘Douglas Farm’ of 470 acres granted in 1799. Moore was the Colony’s master boatbuilder but undertook timber cutting and farming on his farm. The property was later purchased by Dr Robert Wardell and incorporated into his Petersham Estate. Much of the lower ground on the western side of

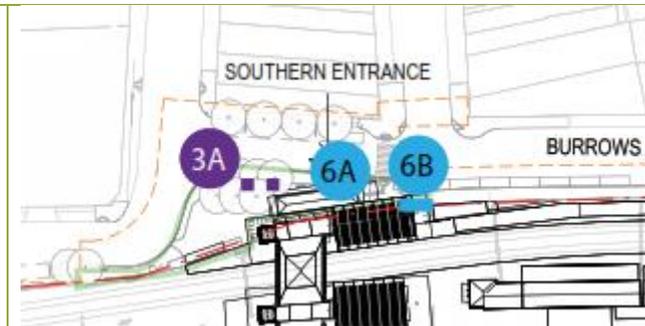


Figure 42. Location of device 3A.

	<p>Sydenham was divided into small farms but following the opening of the railway line and construction of Sydenham (then known as Marrickville) Station in 1884 the lands on both sides of the line were subdivided for residential development. Development west of the line failed due to flooding while the subdivision on the eastern side resulted in the creation of a suburb that had convenient access to the City and to the industries that had begun to occupy the western side of the line. In the period after the Second World War Sydenham saw successive waves of immigrants residing here and creating businesses.</p> <p>Example words include:</p> <ul style="list-style-type: none"> ▪ Thomas Moore ▪ timber-getting ▪ Wardell Estate ▪ Sydenham Farms subdivision ▪ Sydenham Farms cottages ▪ orcharding ▪ small-holdings ▪ industry ▪ migration ▪ community
<p>Example visual media</p>	<p>N/A</p>

7.4 Platform 1

It is proposed that the role of the Platform 1 Parcels Office, subject to demolition, be interpreted on this Platform. Opportunities to draw in the key theme of transportation and activities associated with the moving of people and goods from one place to another, and systems for the provision of such movements.

Recommendation 4A

BARRIER SCREEN (construction implementation opportunity)

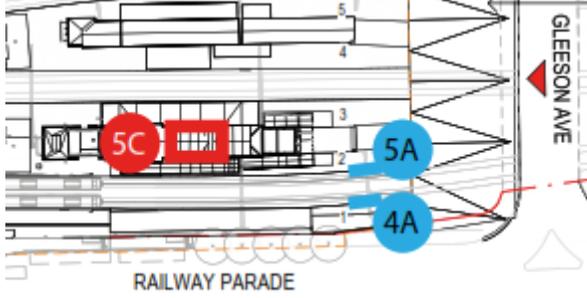
Description	A section of the Platform 1 barrier screen, to be introduced as part of Sydney Metro, would be utilised for an interpretive graphic for the former Parcels Office.	
Location	South-eastern end of Platform 1	 <p>Figure 43. Location of device 4A.</p>
Theme	Goods and People Transport	
Specification	<ul style="list-style-type: none"> ▪ Glazed screen etch film or similar (1200w x 1700h) 	
Limitations	<ul style="list-style-type: none"> ▪ Must not interfere with wayfinding signage and advertising panels ▪ Interpretation here cannot interfere with the movement of people ▪ Must include simple graphics that people can casually survey as they move along the platform 	
Example text	The former Parcels Office formed part of a network operated by NSW Government Railways that couriered parcels throughout the metropolitan area by parcel van. Goods from outside the metropolitan area were carried in brake vans. Parcels carried by the railway system carried stamps coloured according to duties payable and over-stamped with destination.	



Figure 44. Photograph of Parcels Office (Extent Heritage, 2018).

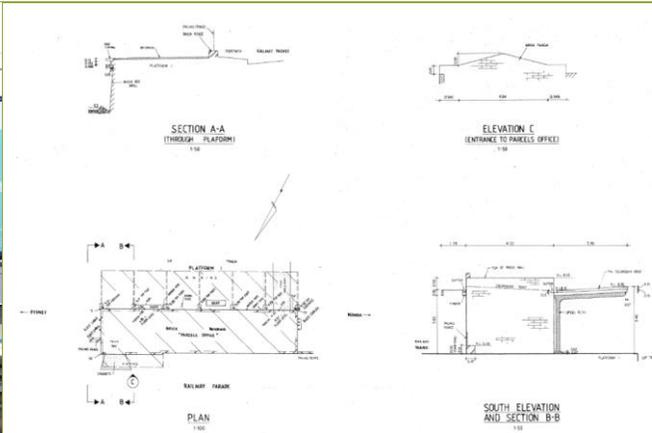


Figure 45. Plan of Platform 1 Parcels Office (Sydney Trains Plan Room, EMD CV0103364).

**Example
visual media**



Figure 46. NSWGR Parcel Stamp with destination.

Recommendation 4B

GLAZED SCREEN (construction implementation opportunity)	
Description	A glazed screen to the South Plaza station entry, to be introduced as part of Sydney Metro, would be utilised for an interpretive graphic.
Location	Centre of Platform 1 near North Plaza station entry
Theme	Goods and People Transport
Specification	<ul style="list-style-type: none"> ▪ Glazed screen etch film or similar (1200w x 1700h)
Limitations	<ul style="list-style-type: none"> ▪ Must not interfere with wayfinding signage and advertising panels ▪ Interpretation here cannot interfere with the movement of people ▪ Must include simple graphics that people can casually survey as they move through the station entrance
Example text	N/A—no text other than potential text associated with a graphic.

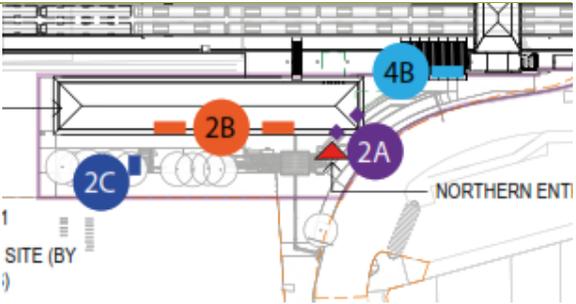


Figure 47. Location of device 4B.



Figure 48. Sydenham-Botany Goods Line (Neil Pollard).

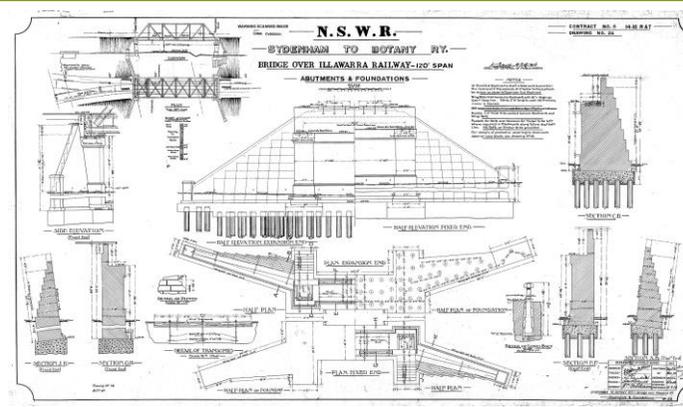
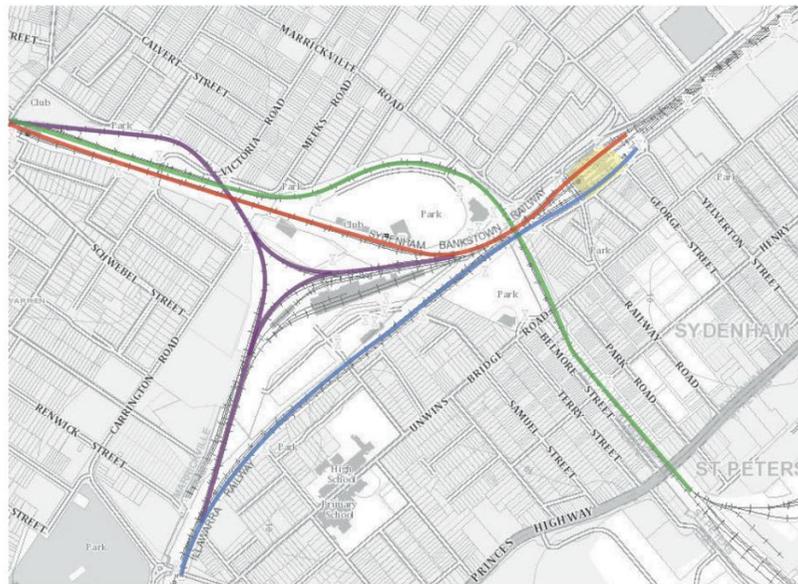


Figure 49. 1915 construction drawing for the Botany Goods Line overbridge (Sydney Trains Plan Room, CV0066151).

**Example
Visual
Media**



- Sydenham Station
- Illawarra Railway 1884
- Bankstown Railway 1876
- Sydenham - Botany goods line 1925
- Tempe Triangle 1920s

7.5 Platform 2/3

This location presents the opportunity to interpret the original alignment of Platform 2 (subject to modification) and activities associated with the moving of people and goods from one place to another, and systems for the provision of such movements.

Works to the Platform 2/3 building associated with the upgrade for Sydney Metro use also present the opportunity for the reintroduction of historic colour schemes.

Works to the Gleeson Avenue overbridge present the opportunity to interpret the early tram line as a key theme of transportation and activities associated with the moving of people and goods from one place to another, and systems for the provision of such movements. Opportunities for interpretation on the Gleeson Avenue overbridge itself are limited. There is low foot traffic along the south western footpath and pedestrians move quickly into the station concourse on the north eastern footpath.

Recommendation 5A

BARRIER SCREEN (construction implementation opportunity)	
Description	A section of the Platform 2/3 barrier screen, to be introduced as part of Sydney Metro, would be utilised for an interpretive graphic.
Location	South eastern end of Platform 2/3
Theme	Gleeson Avenue Overbridge—transformation from trams to buses
Specification	<ul style="list-style-type: none"> Glazed screen etch film or similar (1200w x 1700h)
Limitations	<ul style="list-style-type: none"> Must not interfere with wayfinding signage and advertising panels

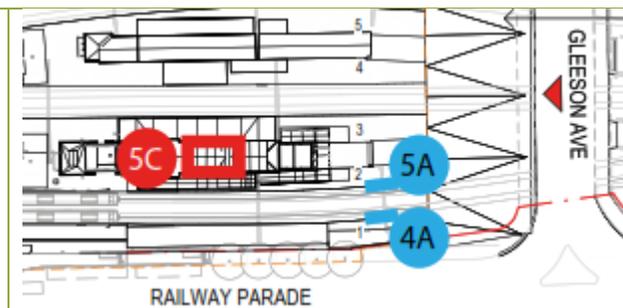


Figure 50. Location of device 5A.

- Interpretation here cannot interfere with the movement of people
- Must include simple graphics that people can casually survey as they move along the platform

Example text

Sydenham Station became an important public transport interchange from the 1920s onwards. Construction of Gleeson Avenue and the Railway Parade embankments allowed trams to cross the railway line to form a connection between Dulwich Hill, Marrickville and the Princes Highway. Bus connections were later added to transport hub with a bus stand located in Burrows Avenue.

Example visual media



Figure 51. A tram on the Gleeson Avenue overbridge (<https://tdu.to/i/41412>).

Recommendation 5B

STEEL MESH SCREEN X 1 (construction implementation opportunity)	
Description	A section of the Platform 2/3 barrier screen, to be introduced as part of Sydney Metro, would be utilised for an interpretive graphic.
Location	North western end of Platform 2/3
Theme	Transport Hub
Specification	<ul style="list-style-type: none"> 1 x screen etching on 1200w x 2400h and 1200m aluminium cladding panel
Limitations	<ul style="list-style-type: none"> Must not interfere with wayfinding signage and advertising panels Interpretation here cannot interfere with the movement of people Must include simple graphics that people can casually survey as they move along the platform
Example text	N/A—no text.



Figure 52. Location of device 5B.

**Example
Visual Media**



Figure 53. New South Wales Government Railways C32-Class Locomotive 3267, Sydenham Station, [n.d.]. View from former concourse above Platform 5 looking across Platform 6 (Australian Railway Historical Society, New South Wales Division).

Recommendation 5C

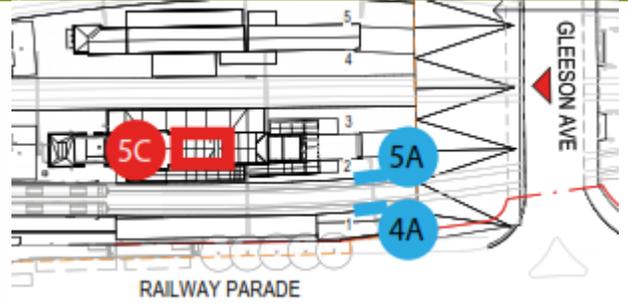
HERITAGE RESTORATION—PAINT SCHEME, BENCHES AND LIGHTING (construction implementation opportunity)	
Description	Using the Platform 2/3 waiting room, this interpretation device would see the reinstatement of an appropriate historic paint scheme and reintroduction of period appropriate waiting room benches and lighting. The work would need to be undertaken in accordance with the results of a Historic Paint Test and the ESB 010 Heritage Paint Schemes standard. In addition, the waiting room bench and lighting design would need to be prepared in liaison with a heritage specialist.
Location	<p>Waiting Room for Platform 2/3 Station Building</p>  <p>Figure 54. Location of device 5C.</p>
Theme	Goods and People Transport
Specification	<ul style="list-style-type: none"> ▪ Repaint interior, including waiting room benches. ▪ Reinstatement of period appropriate waiting room benches, to a heritage specialist approved design. ▪ ESB 010 Heritage Paint Schemes. ▪ Refer to 'Internal Paint Colour Schemes and Decorative Finishes' (2000) prepared by Otto Cserhalmi + Partners Pty Ltd. ▪ Capture in Scope of Conservation Works being delivered as part of Sydney Metro.
Limitations	<ul style="list-style-type: none"> ▪ Heritage Paint Testing required to determine appropriate paint scheme.
Example text	N/A—no text.
Example visual media	N/A—no visual media.



Figure 55. Waiting room in Platform 2/3 Station Building.

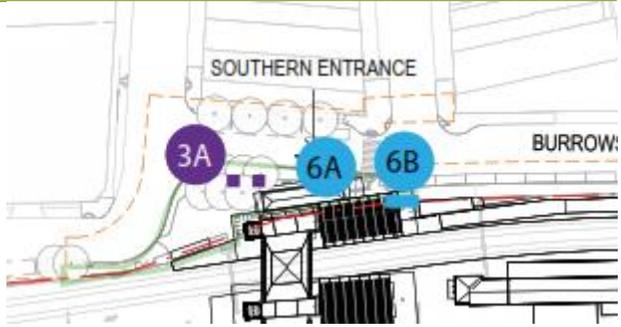


Figure 56. Waiting room bench.

7.6 Platform 6

This area is considered to be suitable to an interpretation of the changing layout of the site over time.

Recommendation 6A

WALL SCREEN (construction implementation opportunity)	
Description	<p>A wall screen depicting the development of the railway station site either in:</p> <ul style="list-style-type: none"> • Plan view to show changes in the platform/building configurations • Elevation view to show the development of platform canopies in the site. The site contains a range of platform canopies that speak to the way the platforms and lines were altered over time.
Location	<p>Platform 6</p>  <p>Figure 57. Location of device 6A.</p>
Theme	Goods and People Transport
Specification	<ul style="list-style-type: none"> ▪ Glazed screen etch film or similar (1200w x 1700h)
Limitations	<ul style="list-style-type: none"> ▪ Little opportunity for text content ▪ Must not interfere with wayfinding signage and advertising panels ▪ Interpretation here cannot interfere with the movement of people ▪ Must include simple graphics that people can casually survey as they move around the station
Example text	N/A—no text other than that shown on example graphic.

**Example
 visual
 media**

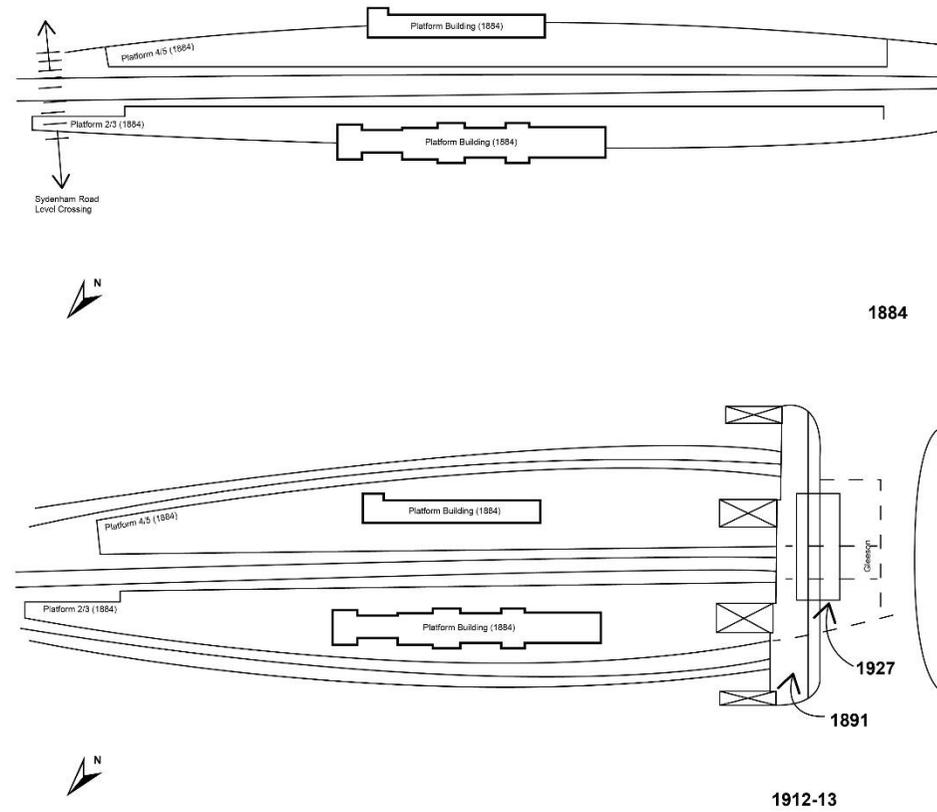


Figure 58. Sketch of site development in 1884 and 1912–1913 (Extent Heritage and HASSELL + Weston Williamson, 2018).

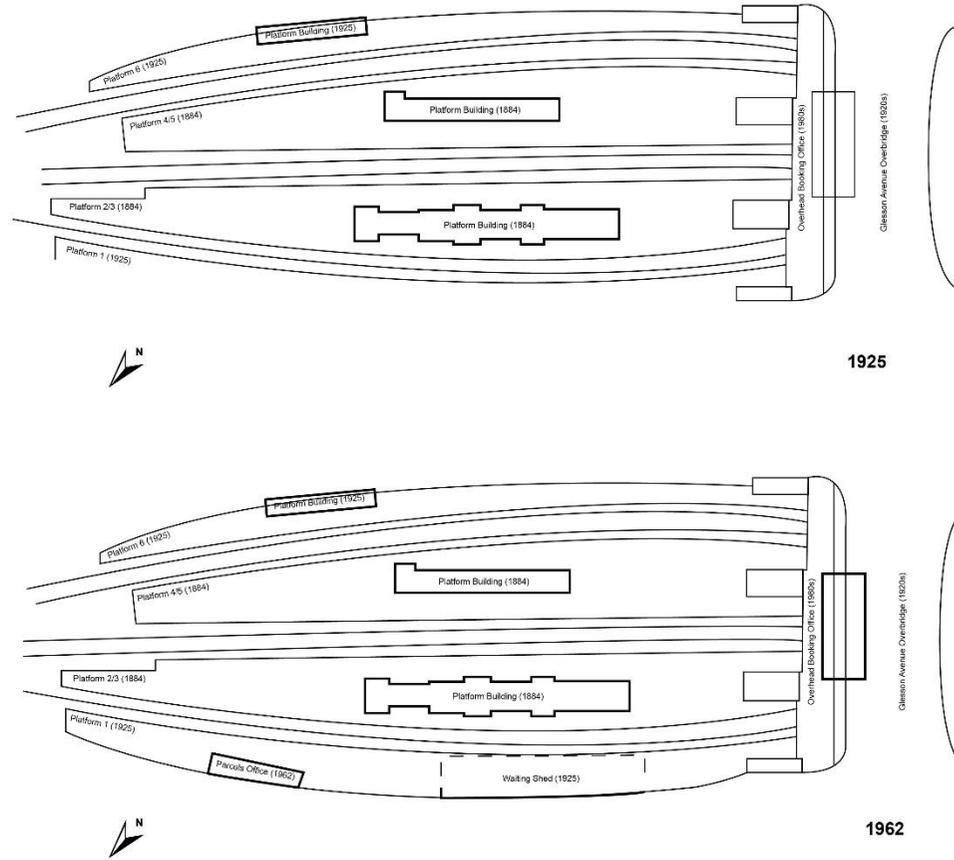


Figure 59. Sketch of site development in 1925 and 1962 (Extent Heritage and HASSELL + Weston Williamson, 2018).

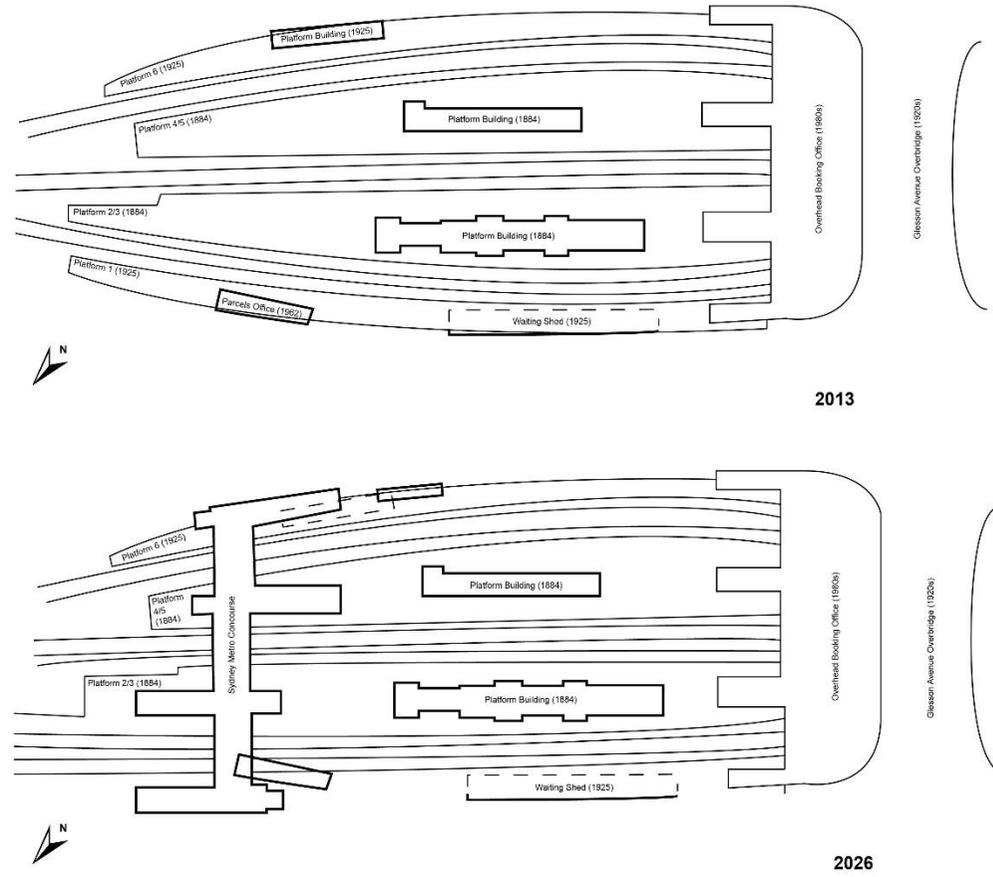
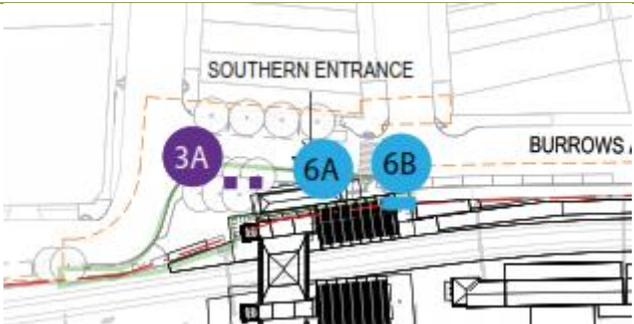


Figure 60. Sketch of site development in 2013 and 2026 (Extent Heritage and HASSELL + Weston Williamson, 2018).

Recommendation 6B

GLAZED SCREEN (construction implementation opportunity)	
Description	A glazed screen to the South Plaza station entry, to be introduced as part of Sydney Metro, would be utilised for an interpretive graphic.
Location	South Plaza  <p>Figure 61. Location of device 6B.</p>
Theme	Goods and People Transport
Specification	<ul style="list-style-type: none"> ▪ Glazed screen etch film or similar (1200w x 1700h)
Limitations	<ul style="list-style-type: none"> ▪ Must not interfere with wayfinding signage and advertising panels ▪ Interpretation here cannot interfere with the movement of people ▪ Must include simple graphics that people can casually survey as they move through the station entrance
Example text	Sydenham Station, 1910. View from present platform 6 showing Platform 4/5 (right) and Platform 2/3 centre.

**Example
visual media**



SYDENHAM RAILWAY STATION.

Figure 62. Sydenham Station 1910. View from present platform 6 showing Platform 4/5 (right) and Platform 2/3 centre (Marrickville Image Library rhpc007).

8. Conclusions and Recommendations

8.1 Conclusions

This Heritage Interpretation Plan (HIP) has considered the Sydenham Metro Upgrade Project as the canvas for several forms of heritage interpretation. Utilising the existing and upgraded station platform areas, the new north and south plazas, and the Sydenham Pit and Drainage Pumping Station, interpretation devices aim to use traditional and contemporary modes of interpretation to create a diverse interpretive environment. This interpretation plan will be followed by a detailed design and implementation program which will be delivered as part of the construction package for Sydenham Metro Upgrade Project.

8.2 Recommendations

The following general recommendations are noted:

- As this HIP has been prepared prior to the archaeological program that is tied to the construction package, any significant archaeological remains will need to be assessed for inclusion during the interpretation delivery stage of the project, as appropriate;
- Out of courtesy, the RAPs that responded to the consultation request should be given an opportunity to review the final text content prior to interpretation implementation.
- Further information on the public art program should be sought prior the interpretation implementation program to ensure that public art and interpretation are well integrated at the site.
- It is recommended that as part of the interpretation implementation program, the three existing interpretation panels within the current concourse are relocated to a more appropriate location within the concourse which will allow more active engagement with the device.
- A Historic Paint Analysis must be undertaken by a qualified heritage expert to determine the most appropriate paint scheme for the Platform 2/3 Waiting Room.
- As the interpretation devices will not diminish the cultural value for which the sites are listed, there are no other specific recommendations for the care of heritage fabric.

8.3 Management of interpretation installations

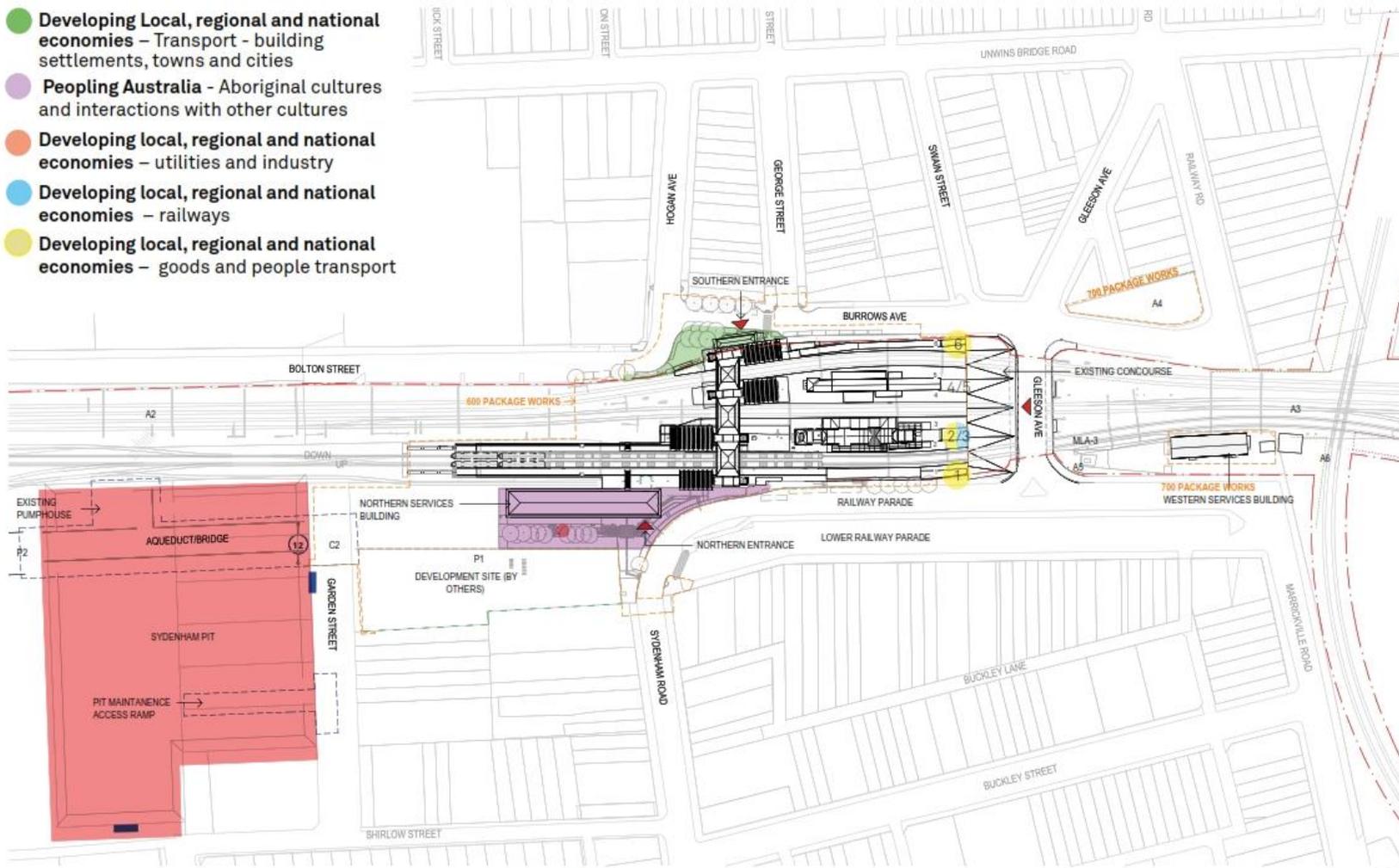
The physical interpretive works and infrastructure proposed in this study are intended as self-guided, physically robust and secure elements that will require minimal ongoing supervision and maintenance. The proposed interpretation and infrastructure are intended to have a physical/technological lifespan of approximately 15+ years.

Ongoing inspection of interpretive works should be conducted on a 12 monthly basis to review element condition, conservation conditions and security. As some of the interpretation will be located in outdoor public spaces, they may require occasional maintenance or replacement due to the effects of UV exposure, vandalism and accidental damage.

Appendix 1: Heritage Interpretation Design Media

THEMATIC PLAN

- **Developing Local, regional and national economies** – Transport - building settlements, towns and cities
- **Peopling Australia** - Aboriginal cultures and interactions with other cultures
- **Developing local, regional and national economies** – utilities and industry
- **Developing local, regional and national economies** – railways
- **Developing local, regional and national economies** – goods and people transport



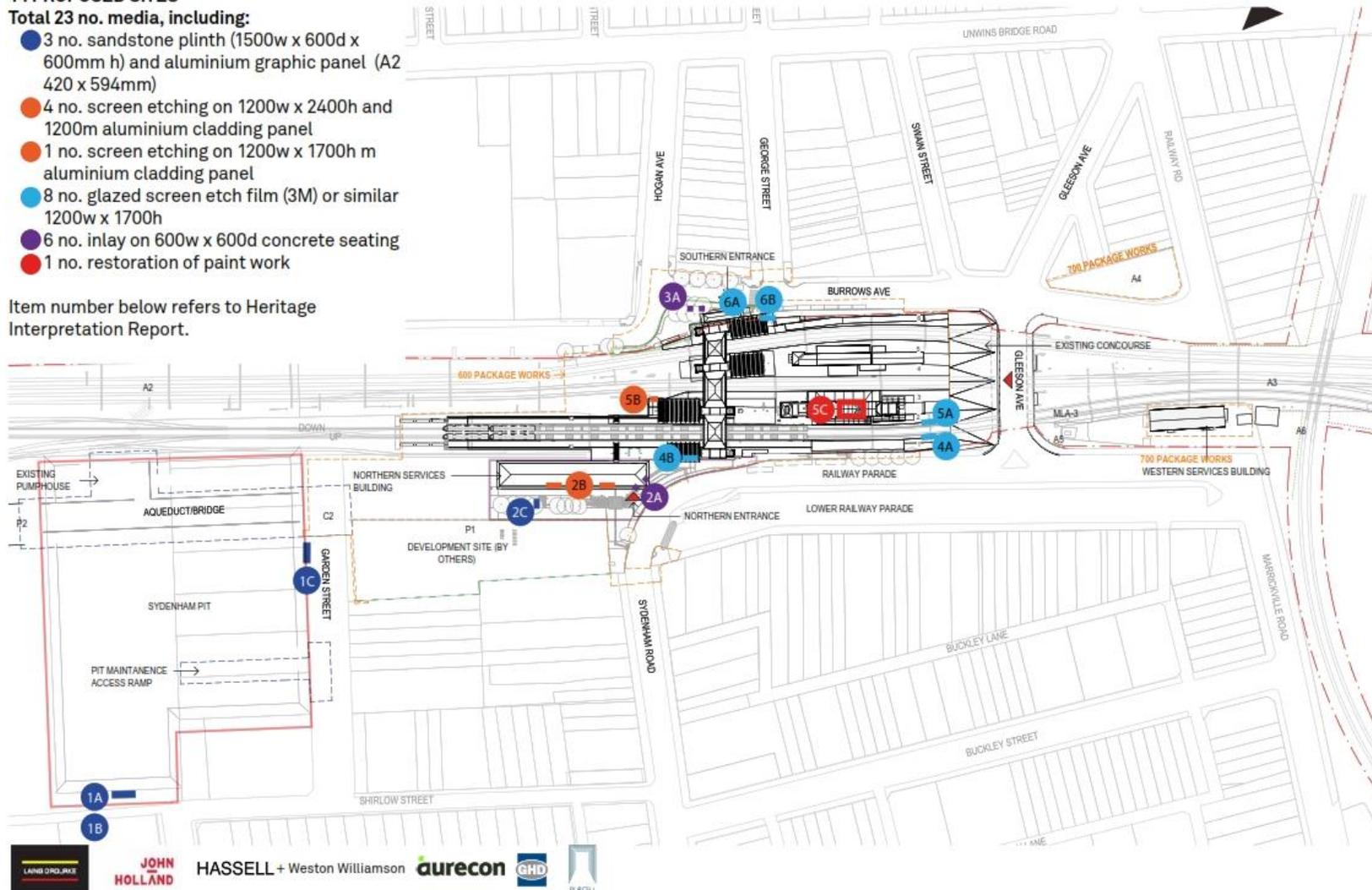
HIP MEDIA - PROPOSED SUMMARY

14 PROPOSED SITES

Total 23 no. media, including:

- 3 no. sandstone plinth (1500w x 600d x 600mm h) and aluminium graphic panel (A2 420 x 594mm)
- 4 no. screen etching on 1200w x 2400h and 1200m aluminium cladding panel
- 1 no. screen etching on 1200w x 1700h m aluminium cladding panel
- 8 no. glazed screen etch film (3M) or similar 1200w x 1700h
- 6 no. inlay on 600w x 600d concrete seating
- 1 no. restoration of paint work

Item number below refers to Heritage Interpretation Report.



PROPOSED MEDIA A
Sandstone plinth and aluminium graphic panel

Proposed Locations

- _ 1A - Shirlow Street lookout
- _ 1C - Garden Street
- _ 2C - Railway Parade Northern Plaza

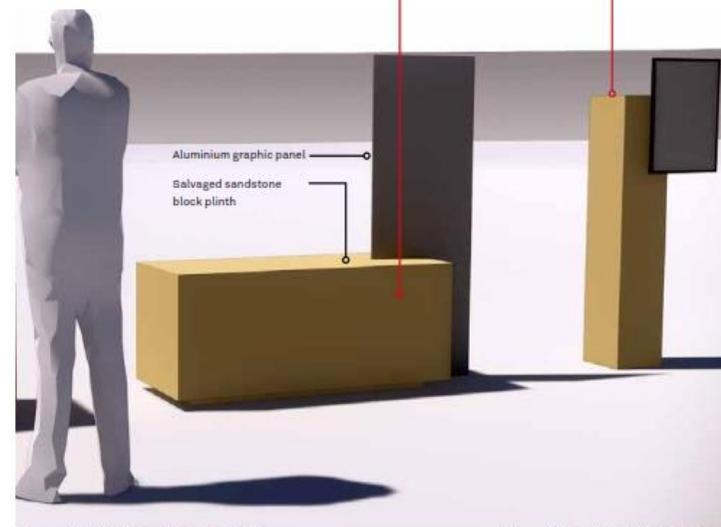
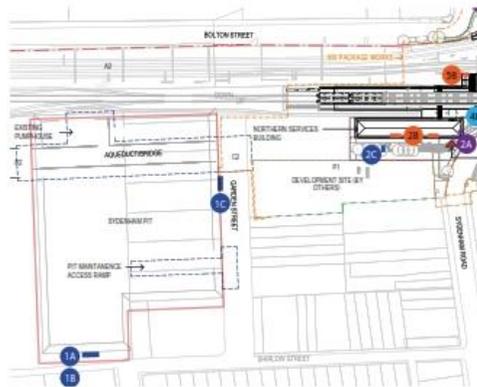
Quantity

- _ 3 no

Proposed Design

Provide a tactile experience that connects the viewer with the pit wall production technique deployed by the unskilled labourers in the construction of the pit wall in 1920s.

- _ Plinth to be consists of salvaged sandstone block from pit wall. (Dim:)
- _ Aluminium graphic panel to be in powder coated finish (dim:)



Item 1A & 1C _ Plinth with vertical plaque

Item 2C _ Vertical totem

PROPOSED MEDIA BA ●
Perforated wall cladding screen etching
Type BA - Flat Screen

Proposed Locations

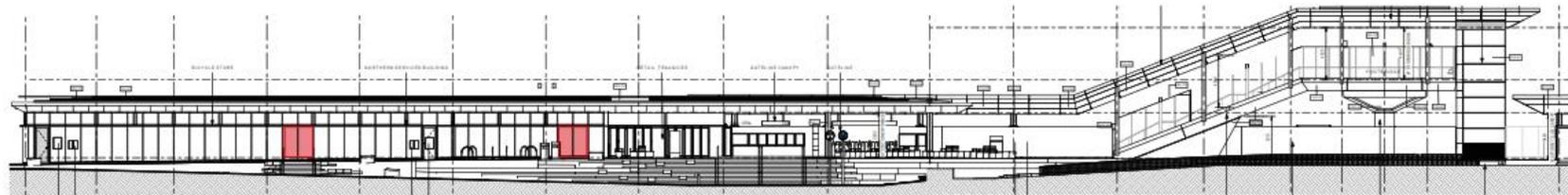
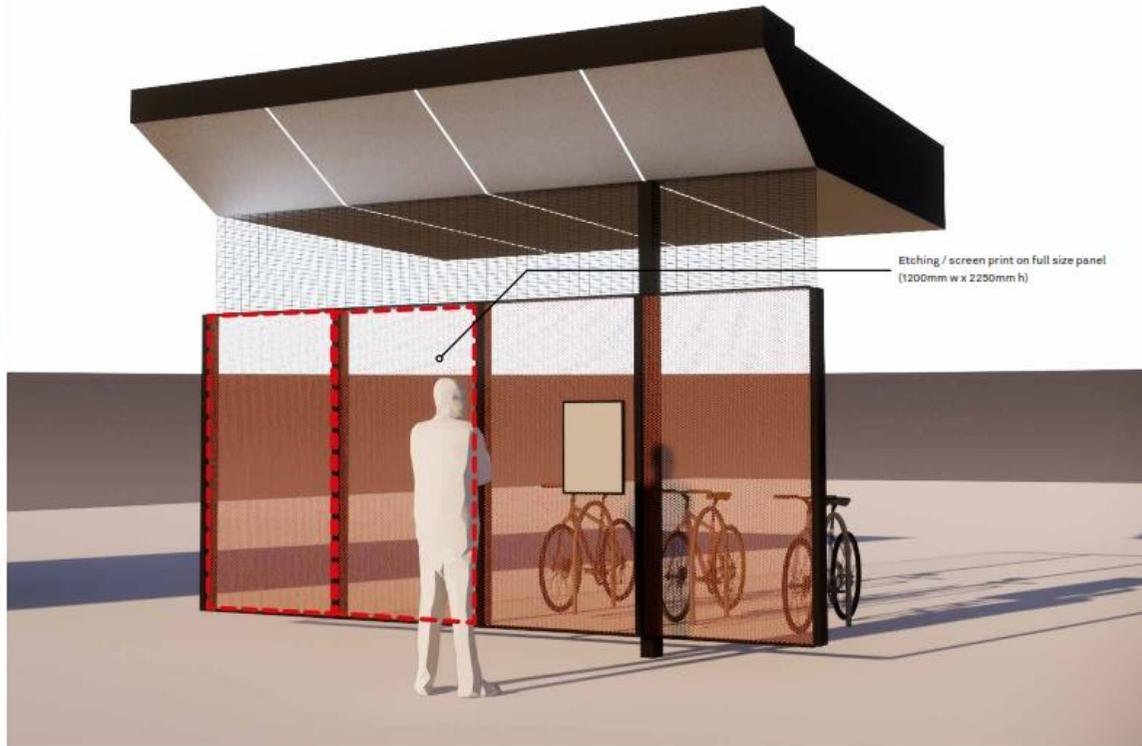
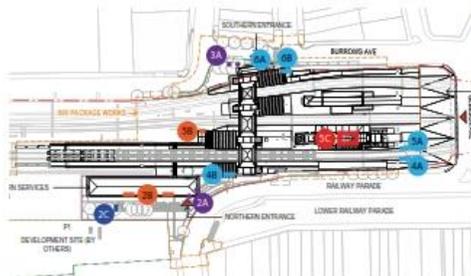
- _ 2B - Northern plaza NSB bike store cladding

Quantity

- _ 4 no screen (1200mm w x 2400mm h)

Proposed Design

- _ A2 aluminium graphic panel; or
- _ Etching / silk screen print on full sized panel (1200mm w x 2250mm h)



Northern plaza - NSB north elevation



PROPOSED MEDIA BB ●
Perforated wall cladding screen etching
Type BB - Flat platform barrier screen

Proposed Locations

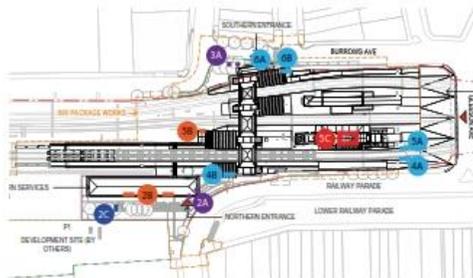
- _ 5B - Platform 4 city end, adjacent to stair landing

Quantity

- _ 1 no screen (1200mm w x 1700mm h)

Proposed Design

- _ Etching / silk screen print on full sized panel



PROPOSED MEDIA CA
Glazed screen applied graphic film (3M film)
Type CA- 1200w x 2400h

Proposed Locations

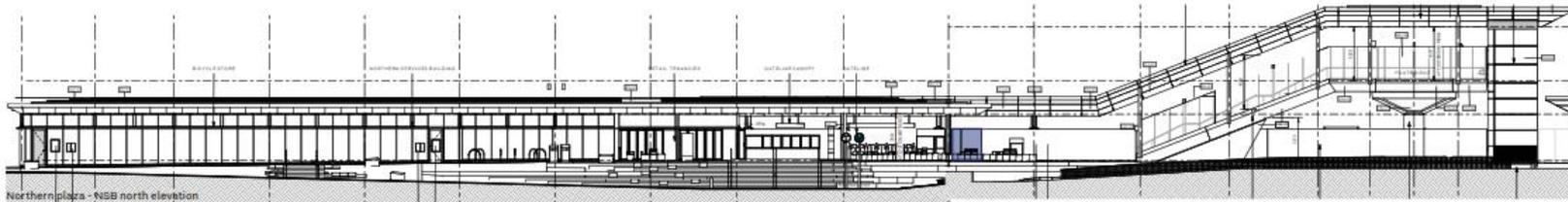
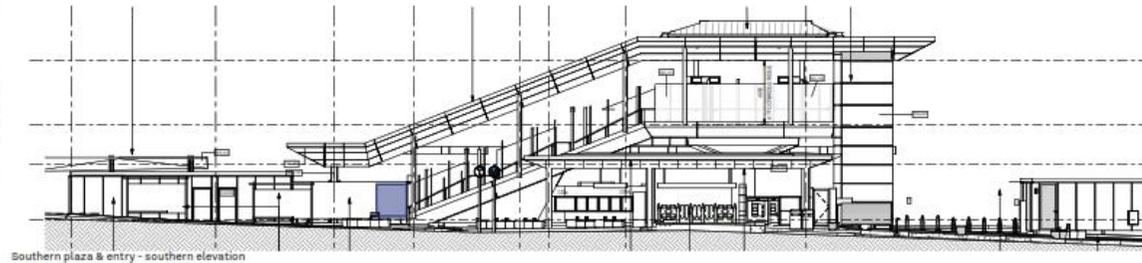
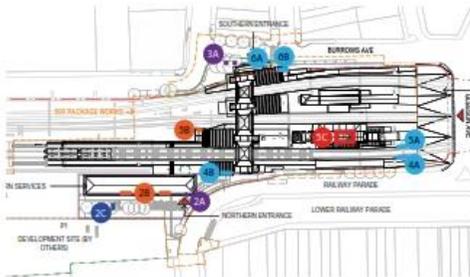
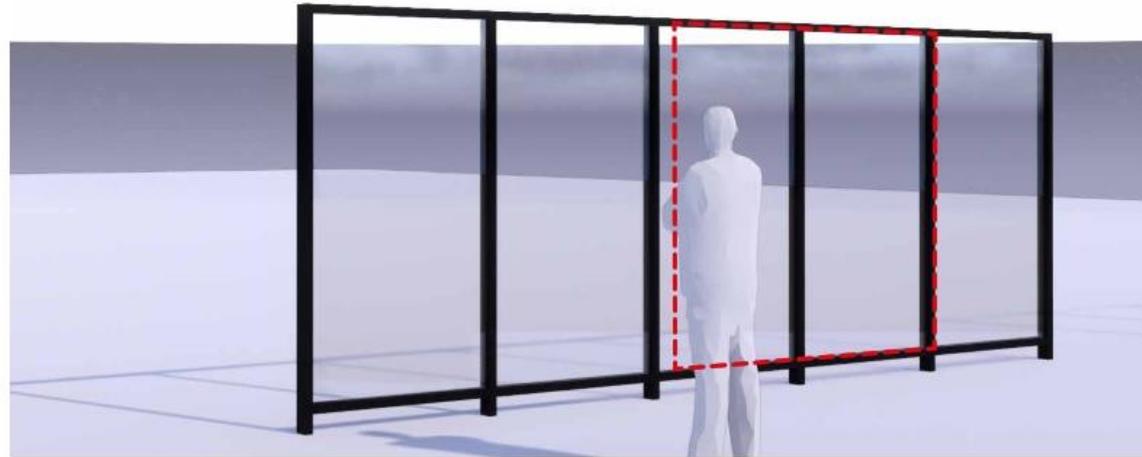
- _ 4B - Northern entry plaza glazed fence
- _ 6A - Southern entry plaza glazed fence

Quantity

- _ 4 no screens (1200mm w x 1750mm h)

Proposed Design

- _ Graphical transparent film or similar to be applied to full size glass panel.
- _ Final graphic application to be confirmed subject to maintenance review.



PROPOSED MEDIA CB
Glazed screen applied graphic film (3M film)
Type CB - 1200w x 1700h

Proposed Locations

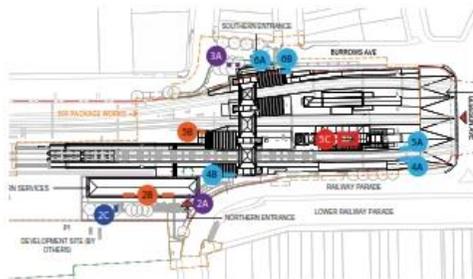
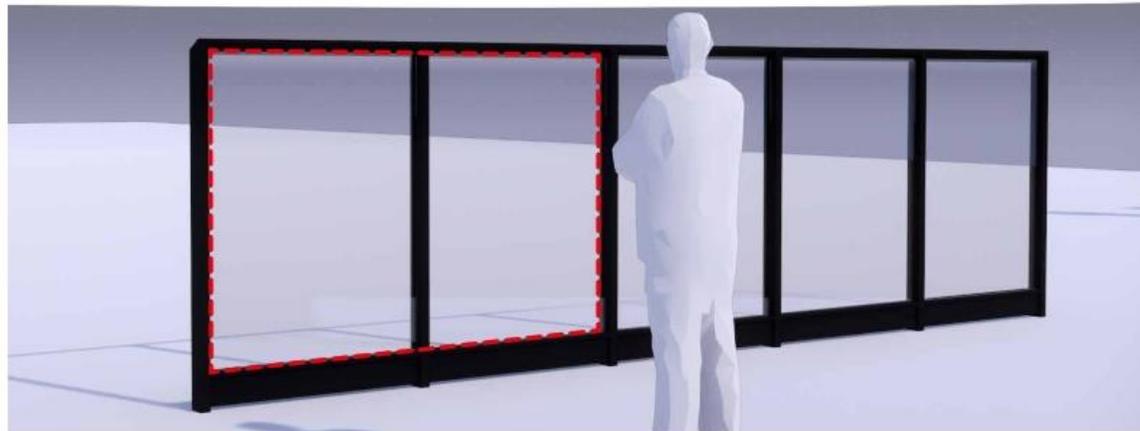
- _ 4A - 2 no screens on Platform 1 country end
- _ 5A - 2 no screens on Platform 2 country end

Quantity

- _ 4 no screen (1200mm w x 1700mm h)

Proposed Design

- _ Graphical transparent film to be applied to full size glass panel
- _ Final graphic application to be confirmed subject to maintenance review.



PROPOSED MEDIA 4
Precast Concrete Seating Inlay Graphics

Proposed Locations

- _ 2A - Northern entry plaza
- _ 3A - Southern entry plaza

Quantity

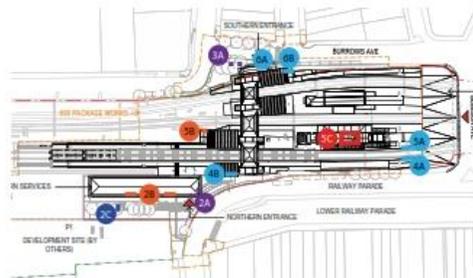
- _ 6 no seats (600mm w x 600mm d)

Proposed Design

- _ Graphical inlay, fabricated using silicon form liner in concrete seat formwork



Example of silicon formliner



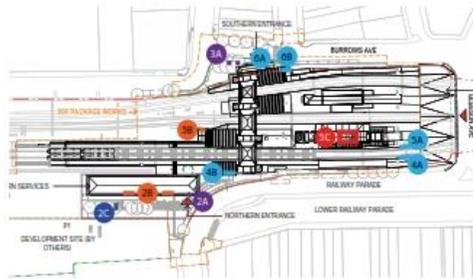
PROPOSED MEDIA 5 ●
Reinstatement of original internal paint scheme

Proposed Locations

- _ Platform 2/3 heritage building waiting room

Quantity

- _ 1 room



HASSELL + Weston Williamson



Appendix 2: Aboriginal Community Consultation

Appendix 2.1: Aboriginal Community Consultation Log

A log of actions and correspondence regarding Aboriginal community consultation undertaken in the preparation of this HIP is provided in the following pages.

Organisation/Group	Representative	Date	Comments	Extent Heritage Contact
Distribution of HIP to Aboriginal Parties				
Metropolitan Local Aboriginal Land Council	Nathan Moran	14.11.18	Emailed a copy of the Heritage Interpretation Plan (HIP) to the RAPS, and requested their feedback on the Aboriginal cultural value and stories of Sydenham.	Laressa Barry
Darug Land Observations	Gordon Workman			
Darug Land Observations	Jamie Workman			
Tocomwall	Scott Franks		Requested feedback on specific themes or research avenues not covered in the HIP. Feedback requested by 12 December 2018.	
Woronora Plateau Gundangara Elders Council	Kayla Williamson			
Murra Bidgee Mullangari Aboriginal Corporation	Darleen Johnson			
Aboriginal Archaeology Service Inc.	Tony Williams			
Aboriginal Archaeology Service Inc.	Andrew Williams			
Gundungurra Tribal Technical Services	Christopher Payne			
Gundungurra Tribal Technical Services	David Bell			
Gundungurra Tribal Technical Services	Peter Foster			
Bilinga Cultural Heritage Technical Services	Robert Brown			
Gunyuu Cultural Heritage Technical Services	Darlene Hoskins-McKenzie			
Munyunga Cultural Heritage Technical Services	Suzannah McKenzie			
Murrumbil Cultural Heritage Technical Services	Levi McKenzie-Kirkbright			
Wingikara Cultural Heritage Technical Services	Wandai Kirkbright			
DJMD Consultancy	Darren Duncan			
Aboriginal Heritage Office	David Watts			

Organisation/Group	Representative	Date	Comments	Extent Heritage Contact
Gandangara Local Aboriginal Land Council	Rongu Puketapu			
KYWG	Phil Khan	14.11.2018	Requested that a hard copy be sent in the mail.	Laressa Barry
Darug Aboriginal Cultural Heritage Assessments	Celestine Everingham	15.11.2018	Posted a hard copy of the Draft HIP, with a brief covering letter, requesting feedback on the Aboriginal cultural value and stories of Sydenham.	Laressa Barry
KYWG	Phil Khan		Requested feedback on specific themes or research avenues not covered in the HIP.	
			Feedback requested by 12 December 2018.	
Metropolitan Local Aboriginal Land Council	Nathan Moran	10.12.2018	Provided follow up reminder of the imminent finalisation of the HIP, and requested feedback.	Laressa Barry
Darug Land Observations	Gordon Workman			
Darug Land Observations	Jamie Workman			
Tocomwall	Scott Franks			
Woronora Plateau Gundangara Elders Council	Kayla Williamson			
Murra Bidgee Mullangari Aboriginal Corporation	Darleen Johnson			
Aboriginal Archaeology Service Inc.	Tony Williams			
Aboriginal Archaeology Service Inc.	Andrew Williams			
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Gundungurra Tribal Technical Services	David Bell			
Gundungurra Tribal Technical Services	Peter Foster			
Bilinga Cultural Heritage Technical Services	Robert Brown			
Gunyu Cultural Heritage Technical Services	Darlene Hoskins-McKenzie			

Organisation/Group	Representative	Date	Comments	Extent Heritage Contact
Munyunga Cultural Heritage Technical Services	Suzannah McKenzie			
Murrumbil Cultural Heritage Technical Services	Levi McKenzie-Kirkbright			
Wingikara Cultural Heritage Technical Services	Wandai Kirkbright			
DJMD Consultancy	Darren Duncan			
Aboriginal Heritage Office	David Watts			
Gandangara Local Aboriginal Land Council	Rongu Puketapu			
KYWG	Phil Khan	10.12.2018	Phil called to state that he was happy with the contents and the intent of the Interpretation Plan but was unable to offer any additional information or advice. He noted that the original Aboriginal inhabitants of the Redfern (by extension, Sydenham) area were now gone, and that Darug people were now coming in to the area. He suggested that Sydney Metro hold a meeting with local Aboriginal people in the Redfern area.	Laressa Barry
Aboriginal Archaeology Service Inc.	Andrew Williams	10.12.2018	Andrew emailed to say that AAS agreed with the documentation but was unable to provide any further comments on the questions supplied. He recommended that Sydney Metro talk with the Metropolitan Local Aboriginal Land Council.	Laressa Barry
Metropolitan Local Aboriginal Land Council	Selina Timothy	11.12.2018	Sent HIP directly to Metropolitan LALC Cultural Heritage Officer, in case the report had not been forwarded from the main Metro email.	Laressa Barry
KYWG	Stefeanie	12.12.2018	Stefeanie provided a written response in support of the HIP.	Laressa Barry

Laressa Barry

From: Laressa Barry
Sent: Wednesday, 14 November 2018 4:19 PM
To: bookings@metrolalc.org.au; Rpuketapu@glalc.org.au; gordow51@bigpond.net.au; Daruglandobservations@gmail.com; scott@tocomwall.com.au; philipkhan.acn@live.com.au; kayla_87@hotmail.com; murrabidgeemullangari@yahoo.com.au; aas.info@bigpond.com; chrispayne776@gmail.com; bilinga@mirramajah.com; gunyuu@mirramajah.com; munyunga@mirramajah.com; murrumbul@mirramajah.com; wingikara@mirramajah.com; darrenjohnduncan@gmail.com; info@aboriginalheritage.org
Cc: Ron.Turner2@transport.nsw.gov.au; Dr Alan Williams; Corinne Softley; Graham Wilson; Laressa Barry
Subject: Sydney Metro City and Southwest: Sydenham Station and Junction Main Works - Distribution of Draft Heritage Interpretation Plan
Attachments: SSJ_HIP_DraftV2.pdf

Dear Registered Aboriginal Stakeholders,

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If you have any comments please send them to me within 28 days, but please by no later than **12 December 2018**. I would be more than happy to chat with you over the phone or come meet with you if that would be easier.



If you have any trouble accessing the Interpretation Plan or require any further information please do not hesitate to contact me via mail, phone or email on the below details.

Kind regards

Laessa Barry | BA (Hons)
Heritage Advisor/Archaeologist
T 02 9555 4000

lbarry@extent.com.au

extent.com.au

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15 November 2018

Darug Aboriginal Cultural Heritage Assessments

Unit 9, 6 Chapman Avenue
Chatswood NSW 2067

**Re: Sydney Metro City and Southwest: Sydenham Station and Junction Main Works -
Distribution of Draft Heritage Interpretation Plan**

Dear Celestine and Gordon,

Thank you for your continued involvement in the Aboriginal cultural heritage assessment components of the Sydney Metro City and Southwest project to date. Back in June of this year, Extent Heritage contacted you to inform you that the John Holland/Laing O'Rourke Joint Venture (JHLORLV) was overseeing the delivery of a small component of the broader project, referred to as the Sydenham Station and Junction Main Works (SSJ).

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Yours Sincerely,



Laressa Barry – Heritage Advisor

15 November 2018

Kamilaroi-Yankuntjatjara Working Group

78 Forbes Street
Emu Plains NSW 2750

Re: Sydney Metro City and Southwest: Sydenham Station and Junction Main Works - Distribution of Draft Heritage Interpretation Plan

Dear Phil,

Thank you for your continued involvement in the Aboriginal cultural heritage assessment components of the Sydney Metro City and Southwest project to date. Back in June of this year, Extent Heritage contacted you to inform you that the John Holland/Laing O'Rourke Joint Venture (JHLORLV) was overseeing the delivery of a small component of the broader project, referred to as the Sydenham Station and Junction Main Works (SSJ).

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Yours Sincerely,



Laressa Barry – Heritage Advisor

Laressa Barry

From: Andrew Williams <aas.info@bigpond.com>
Sent: Monday, 10 December 2018 7:16 PM
To: Laressa Barry
Subject: Re: Sydney Metro City and Southwest: Sydenham Station and Junction Main Works - Distribution of Draft Heritage Interpretation Plan

AAS agrees with documentation. Unsure about questions. Speaking to metro land council .

Regards

Andrew Williams

Sent from my iPhone

On 10 Dec 2018, at 4:07 pm, Laressa Barry <lbarry@extent.com.au> wrote:

Dear Stakeholders,

This is a brief email to remind you that the closing date for commenting on the Heritage Interpretation Plan for Sydenham station is fast approaching, being this coming **Wednesday 12th December**.

If you haven't already, I would greatly appreciate if you could please take a few moments to review the draft plan and provide any suggestions, or ideally provide any responses to the below bullet points.

Thank you for your time, and kind regards,
Laressa

Laressa Barry | BA (Hons)
Heritage Advisor/Archaeologist
T 02 9555 4000
lbarry@extent.com.au
extent.com.au

From: Laressa Barry <lbarry@extent.com.au>
Sent: Wednesday, 14 November 2018 4:19 PM
To: bookings@metrolalc.org.au; Rpuketapu@glalc.org.au; gordow51@bigpond.net.au; Daruglandobservations@gmail.com; scott@tocomwall.com.au; philipkhan.acn@live.com.au; kayla_87@hotmail.com; murrabidgeemullangari@yahoo.com.au; aas.info@bigpond.com; chrispayne776@gmail.com; bilinga@mirramajah.com; gunyuu@mirramajah.com; munyunga@mirramajah.com; murrumbul@mirramajah.com; wingikara@mirramajah.com; darrenjohnduncan@gmail.com; info@aboriginalheritage.org
Cc: Ron.Turner2@transport.nsw.gov.au; Dr Alan Williams <awilliams@extent.com.au>; Corinne Softley <csoftley@extent.com.au>; Graham Wilson <gwilson@extent.com.au>; Laressa Barry <lbarry@extent.com.au>
Subject: Sydney Metro City and Southwest: Sydenham Station and Junction Main Works - Distribution of Draft Heritage Interpretation Plan

Dear Registered Aboriginal Stakeholders,

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Kind regards

Laressa Barry | BA (Hons)
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T 02 9555 4000

lbarry@extent.com.au
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Connect with us on:

<FB_423c721a-4cd0-481a-91f8-780e9f111549.png> <LinkedIn_abc4f2a1-55e1-4efe-8f0e-103053dbb87b.png> <Insta_64ba6810-999a-4a57-a859-66d10d805618.png>

<ExtentLogo_d875f9c6-a53f-4fe6-87a7-19b7d66f4c40.png>

<SSJ_HIP_DraftV2.pdf>

Laressa Barry

From: philip khan <philipkhan.acn@live.com.au>
Sent: Wednesday, 12 December 2018 4:34 PM
To: Laressa Barry
Subject: RE: Sydney Metro City and Southwest: Sydenham Station and Junction Main Works - Distribution of Draft Heritage Interpretation Plan

Hi Laressa,

Thank for sending through your report regarding Sydney Metro City and Southwest: Sydenham Station and Junction Main Works, I agree and support all your recommendations.
Looking forward to working with you in the near future & wishing you and the team a Merry Christmas.

Regards
Stefanie

Kamilaroi Yankuntjatjara Working Group
Aboriginal Cultural Heritage Surveys, Lawn Mowing & Fencing
ABN 33 979 702 507
Not registered for GST
78 Forbes Street, Emu Plains NSW 2750
Mobile: 0434545982
Email: philipkhan.acn@live.com.au

Sent from [Mail](#) for Windows 10

From: Laressa Barry <lbarry@extent.com.au>
Sent: Monday, December 10, 2018 4:07:02 PM
To: bookings@metrolalc.org.au; Rpuketapu@glalc.org.au; gordow51@bigpond.net.au; Daruglandobservations@gmail.com; scott@tocomwall.com.au; philipkhan.acn@live.com.au; kayla_87@hotmail.com; murrabidgeemullangari@yahoo.com.au; aas.info@bigpond.com; chrispayne776@gmail.com; bilinga@mirramajah.com; gunyuu@mirramajah.com; munyunga@mirramajah.com; murrumbul@mirramajah.com; wingikara@mirramajah.com; darrenjohnduncan@gmail.com; info@aboriginalheritage.org
Cc: Ron.Turner2@transport.nsw.gov.au; Laressa Barry
Subject: FW: Sydney Metro City and Southwest: Sydenham Station and Junction Main Works - Distribution of Draft Heritage Interpretation Plan

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T 02 9555 4000

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To: bookings@metrolalc.org.au; Rpuketapu@glalc.org.au; gordow51@bigpond.net.au; Daruglandobservations@gmail.com; scott@tocomwall.com.au; philipkhan.acn@live.com.au; kayla_87_@hotmail.com; murrabidgeemullangari@yahoo.com.au; aas.info@bigpond.com; chrispayne776@gmail.com; bilinga@mirramajah.com; gunyuu@mirramajah.com; munyunga@mirramajah.com; murrumbul@mirramajah.com; wingikara@mirramajah.com; darrenjohnduncan@gmail.com; info@aboriginalheritage.org
Cc: Ron.Turner2@transport.nsw.gov.au; Dr Alan Williams <awilliams@extent.com.au>; Corinne Softley <csoftley@extent.com.au>; Graham Wilson <gwilson@extent.com.au>; Laessa Barry <lbarry@extent.com.au>
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